

LEGEND

| | |
|--|---------------------------------------|
| | NEW NO STOPPING LINES |
| | NEW ROAD MARKINGS |
| | EXISTING ROAD MARKINGS TO BE RETAINED |
| | 4.0m x 0.4m CYCLE LANE SEPARATOR |
| | NEW KERBS |
| | REMOVED KERBS |
| | 50mm HIGH INTERLOCKING SPEED HUMP |

NOTE:

1. IN AREAS OF KERB CUT BACKS OR KERB BUILDOUTS, THE NEW BOLLARDS, KERBS AND PAVING DETAILS ARE TO MATCH THE EXISTING FOOTPATHS.
2. CYCLE LANE DIMENSIONS REFER TO THE CLEAR WIDTH BETWEEN KERB AND THE INSIDE FACE OF THE SEPARATOR.

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| REV | DATE | REVISION DETAILS | APPROVED |
|-----|----------|------------------|-----------|
| 0 | 15/10/23 | FOR CONSTRUCTION | G.MURISON |
| 1 | 26/02/24 | FOR CONSTRUCTION | G.MURISON |
| 2 | 27/03/24 | FOR CONSTRUCTION | G.MURISON |

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|------------------|-------|
| SCALE | 1:250 |
| SIZE | A0 |
| FOR CONSTRUCTION | DATE |
| APPROVED | DATE |

| | | | |
|-------------|--|------|------|
| PROJECT | KARORI TO BOTANIC GARDEN KI PAKAKA CONNECTIONS WCC TRANSITIONAL CYCLEWAYS | | |
| TITLE | GENERAL LAYOUT PRIMARY ROUTE - IFC DESIGNS CHAYTOR STREET & KARORI ROAD SHEET 5 (OF 9) | | |
| DRAWING No. | 522550 | AREA | 0000 |
| ITC | DRG | DGN | 0015 |
| NUMBER | 0015 | REV | 2 |



KARORI ROAD - PART 2



KARORI ROAD - PART 3

NOTE:

1. IN AREAS OF KERB CUT BACKS OR KERB BUILDOUTS, THE NEW BOLLARDS, KERBS AND PAVING DETAILS ARE TO MATCH THE EXISTING FOOTPATHS.
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| | EXISTING ROAD MARKINGS TO BE RETAINED |
| | 4.0m x 0.4m CYCLE LANE SEPARATOR |
| | NEW KERBS |
| | REMOVED KERBS |
| | 50mm HIGH INTERLOCKING SPEED HUMP |



| REV | DATE | REVISION DETAILS | APPROVED | SCALE | SIZE |
|-----|----------|------------------|-----------|-------|------|
| 0 | 27/03/24 | FOR CONSTRUCTION | G MURISON | 1:250 | A0 |

| FOR CONSTRUCTION | APPROVED | DATE |
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| | | |

| PROJECT | TITLE |
|---|---|
| KARORI TO BOTANIC GARDEN KI PAEKAKA CONNECTIONS WCC TRANSITIONAL CYCLEWAYS | GENERAL LAYOUT PRIMARY ROUTE - IFC DESIGNS KARORI ROAD SHEET 6 (OF 9) |

| DRAWING No. | PROJECT No. | AREA | TRC | DGC | NUMBER | REV |
|-------------|-------------|------|-----|------|--------|-----|
| 522550 | 0000 | DRG | DGN | 0016 | 0 | |



LEGEND

| | |
|--|---------------------------------------|
| | NEW NO STOPPING LINES |
| | NEW ROAD MARKINGS |
| | EXISTING ROAD MARKINGS TO BE RETAINED |
| | 4.0m x 0.4m CYCLE LANE SEPARATOR |
| | NEW KERBS |
| | REMOVED KERBS |
| | 50mm HIGH INTERLOCKING SPEED HUMP |

NOTE:

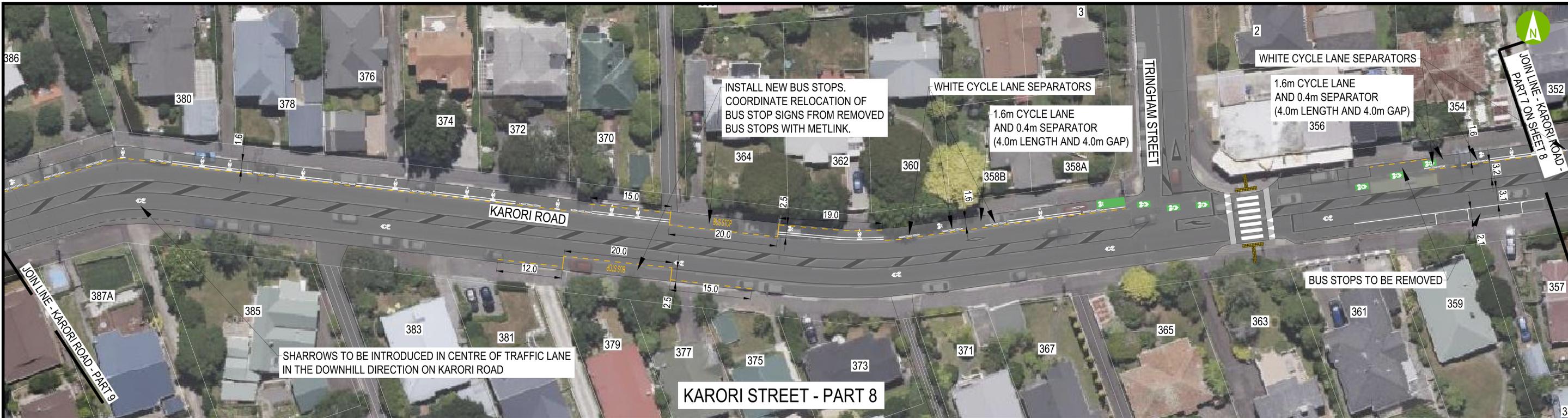
1. IN AREAS OF KERB CUT BACKS OR KERB BUILDOUTS, THE NEW BOLLARDS, KERBS AND PAVING DETAILS ARE TO MATCH THE EXISTING FOOTPATHS.
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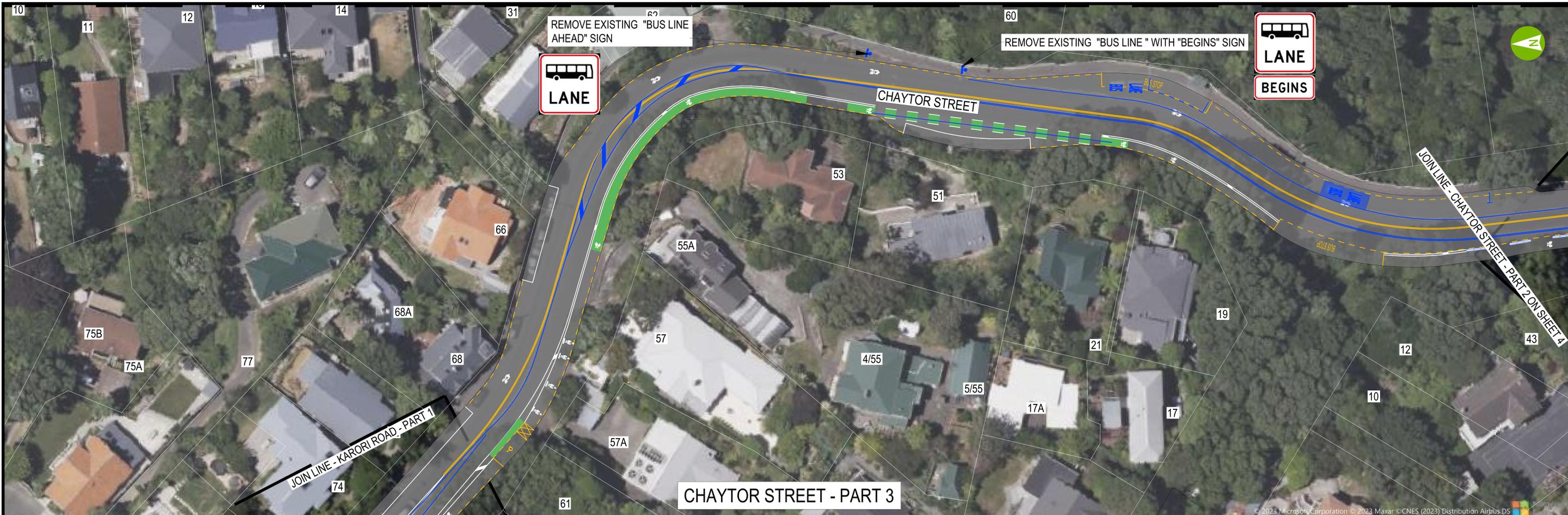
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| REV | DATE | REVISION DETAILS | APPROVED | SCALE | SIZE |
|-----|----------|------------------|-----------|-------|------|
| 0 | 27/03/24 | FOR CONSTRUCTION | G MURISON | 1:250 | A0 |
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| FOR CONSTRUCTION | APPROVED |
|------------------|----------|
| | DATE |

| PROJECT | TITLE |
|--|---|
| KARORI TO BOTANIC GARDEN KAI PĀEKAKA CONNECTIONS WCC TRANSITIONAL CYCLEWAYS | GENERAL LAYOUT PRIMARY ROUTE - IFC DESIGNS KARORI ROAD SHEET 9 (OF 9) |
| DRAWING No. 522550 - 0000 | PROJECT No. 522550 - 0000 |
| AREA 0000 | DATE DRG |
| DATE DGN | NUMBER 0019 |
| REV 0 | REV 0 |



LEGEND

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| | NEW KERBS |
| | REMOVED KERBS |
| | 50mm HIGH INTERLOCKING SPEED HUMP |
| | NEW SIGNAGE |
| | SIGNAGE TO BE REMOVED |

NOTE:

1. ALL SIGNS WORK TO COMPLY WITH THE :NZTA "MANUAL OF TRAFFIC SIGNS AND MARKINGS - PART 1".
2. ALL ROADMARKING TO COMPLY WITH THE :NZTA "MANUAL OF TRAFFIC SIGNS AND MARKINGS - PART 2".
3. ALL SIGN INSTALLATION TO COMPLY WITH WELLINGTON CITY COUNCIL ENGINEERING CODE OF PRACTICE AND NZTA P24.
4. EXACT LOCATION OF SIGNS TO BE CONFIRMED ON SITE WITH 'CLIENTS' ENGINEER REPRESENTATIVE.

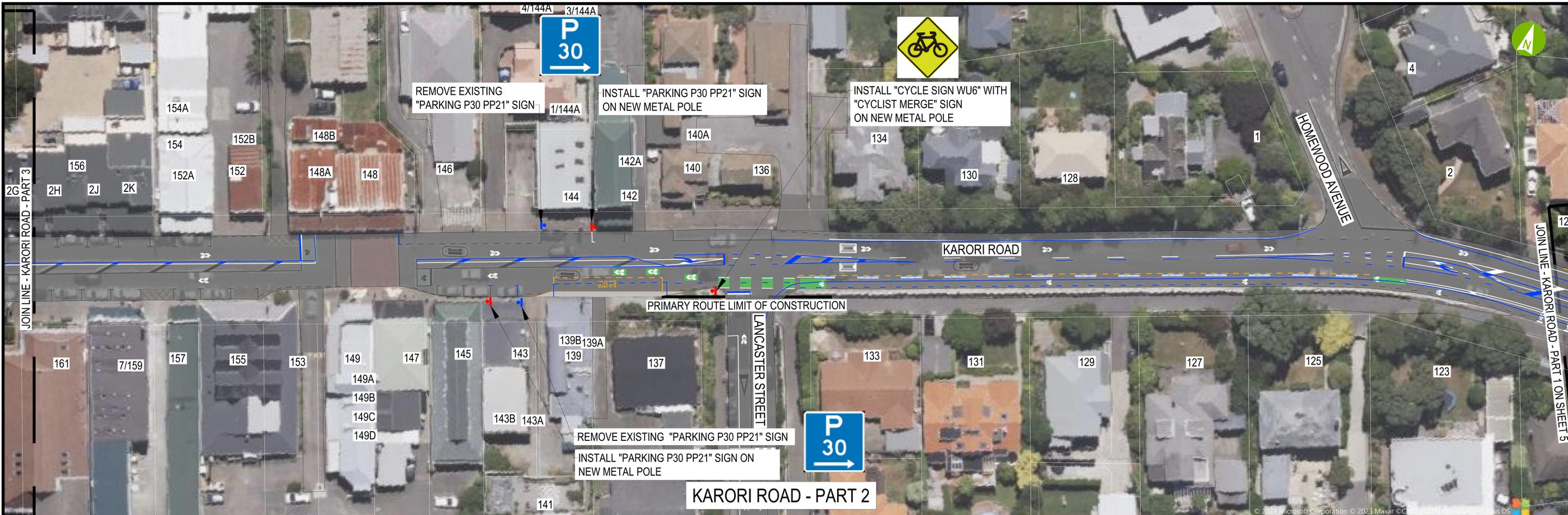
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| REV | DATE | REVISION DETAILS | APPROVED |
|-----|----------|------------------|-----------|
| 0 | 02/02/24 | FOR CONSTRUCTION | G.MURISON |
| 1 | 26/02/24 | FOR CONSTRUCTION | G.MURISON |
| 2 | 27/03/24 | FOR CONSTRUCTION | G.MURISON |

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| SCALE | 1:250 |
| SIZE | A0 |
| FOR CONSTRUCTION | |
| APPROVED | DATE |
| DRAWN | T.PETERS |
| DESIGNED | T.PETERS |
| REVIEWED | M.FRANCIS |

| | | | | |
|-------------|---|------|------|--------|
| PROJECT | KARORI TO BOTANIC GARDEN KI PAKAKA CONNECTIONS WCC TRANSITIONAL CYCLEWAYS | | | |
| TITLE | SIGNAGE PRIMARY ROUTE - IFC DESIGNS CHAYTOR STREET & KARORI ROAD SHEET 5 (OF 9) | | | |
| DRAWING No. | 522550 | AREA | 0000 | DRG |
| | | TTC | DGN | NUMBER |
| | | | | 0115 |
| | | | | REV |
| | | | | 2 |



NOTE:

- ALL SIGNS WORK TO COMPLY WITH THE :NZTA "MANUAL OF TRAFFIC SIGNS AND MARKINGS - PART 1".
- ALL ROADMARKING TO COMPLY WITH THE :NZTA "MANUAL OF TRAFFIC SIGNS AND MARKINGS - PART 2".
- ALL SIGN INSTALLATION TO COMPLY WITH WELLINGTON CITY COUNCIL ENGINEERING CODE OF PRACTICE AND NZTA P24.
- EXACT LOCATION OF SIGNS TO BE CONFIRMED ON SITE WITH 'CLIENTS' ENGINEER REPRESENTATIVE.

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| | NEW KERBS |
| | REMOVED KERBS |
| | 50mm HIGH INTERLOCKING SPEED HUMPS |
| | NEW SIGNAGE |
| | SIGNAGE TO BE REMOVED |

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| REV | DATE | REVISION DETAILS | APPROVED | SCALE | SIZE |
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| 0 | 27/03/24 | FOR CONSTRUCTION | G MURISON | 1:250 | A0 |
| 1 | 29/04/24 | FOR CONSTRUCTION | G MURISON | | |

| APPROVED | DATE |
|----------|------|
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| PROJECT | TITLE |
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| KARORI TO BOTANIC GARDEN KAI PAEKAKA CONNECTIONS WCC TRANSITIONAL CYCLEWAYS | SIGNAGE PRIMARY ROUTE - IFC DESIGNS KARORI ROAD SHEET 6 (OF 9) |

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| DRAWING No. | 522550 |
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| AREA | 0000 |
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| NUMBER | 0116 |
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| REV | 1 |
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- NOTE:**
1. ALL SIGNS WORK TO COMPLY WITH THE NZTA "MANUAL OF TRAFFIC SIGNS AND MARKINGS - PART 1".
 2. ALL ROADMARKING TO COMPLY WITH THE NZTA "MANUAL OF TRAFFIC SIGNS AND MARKINGS - PART 2".
 3. ALL SIGN INSTALLATION TO COMPLY WITH WELLINGTON CITY COUNCIL ENGINEERING CODE OF PRACTICE AND NZTA P24.
 4. EXACT LOCATION OF SIGNS TO BE CONFIRMED ON SITE WITH 'CLIENTS' ENGINEER REPRESENTATIVE.



LEGEND

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| | 4.0m x 0.4m CYCLE LANE SEPARATOR |
| | NEW KERBS |
| | REMOVED KERBS |
| | 50mm HIGH INTERLOCKING SPEED HUMP |
| | NEW SIGNAGE |
| | SIGNAGE TO BE REMOVED |

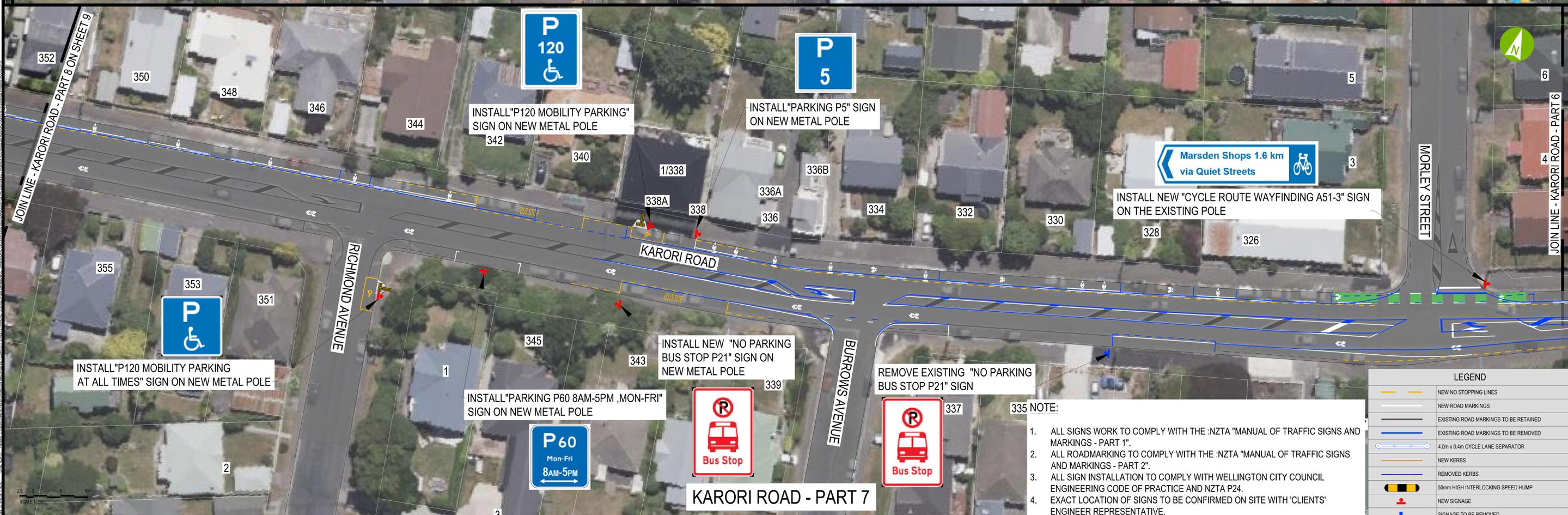


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| REV | DATE | REVISION DETAILS | APPROVED | SCALE | SIZE |
|-----|----------|------------------|-----------|-------|------|
| 0 | 27/03/24 | FOR CONSTRUCTION | G MURISON | 1:250 | A0 |
| 1 | 29/04/24 | FOR CONSTRUCTION | G MURISON | | |

| APPROVED | SCALE | SIZE | FOR CONSTRUCTION |
|-----------|-------|------|------------------|
| G MURISON | 1:250 | A0 | FOR CONSTRUCTION |
| T PETERS | | | APPROVED |
| T PETERS | | | DATE |
| M FRANCIS | | | |

| PROJECT | TITLE | PROJECT No. | AREA | TRC | DGC | NUMBER | REV |
|--|--|-------------|------|-----|-----|--------|-----|
| KARORI TO BOTANIC GARDEN KAI PAEKAKA CONNECTIONS WCC TRANSITIONAL CYCLEWAYS | SIGNAGE PRIMARY ROUTE - IFC DESIGNS KARORI ROAD SHEET 7 (OF 9) | 522550 | 0000 | DRG | DGN | 0117 | 1 |



- 335 NOTE:
1. ALL SIGNS WORK TO COMPLY WITH THE :NZTA "MANUAL OF TRAFFIC SIGNS AND MARKINGS - PART 1".
 2. ALL ROADMARKING TO COMPLY WITH THE :NZTA "MANUAL OF TRAFFIC SIGNS AND MARKINGS - PART 2".
 3. ALL SIGN INSTALLATION TO COMPLY WITH WELLINGTON CITY COUNCIL ENGINEERING CODE OF PRACTICE AND NZTA P24.
 4. EXACT LOCATION OF SIGNS TO BE CONFIRMED ON SITE WITH 'CLIENTS' ENGINEER REPRESENTATIVE.

| LEGEND | |
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| | EXISTING ROAD MARKINGS TO BE RETAINED |
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| | 4.0m x 0.4m CYCLE LANE SEPARATOR |
| | NEW KERBS |
| | REMOVED KERBS |
| | 50mm HIGH INTERLOCKING SPEED HUMPS |
| | NEW SIGNAGE |
| | SIGNAGE TO BE REMOVED |

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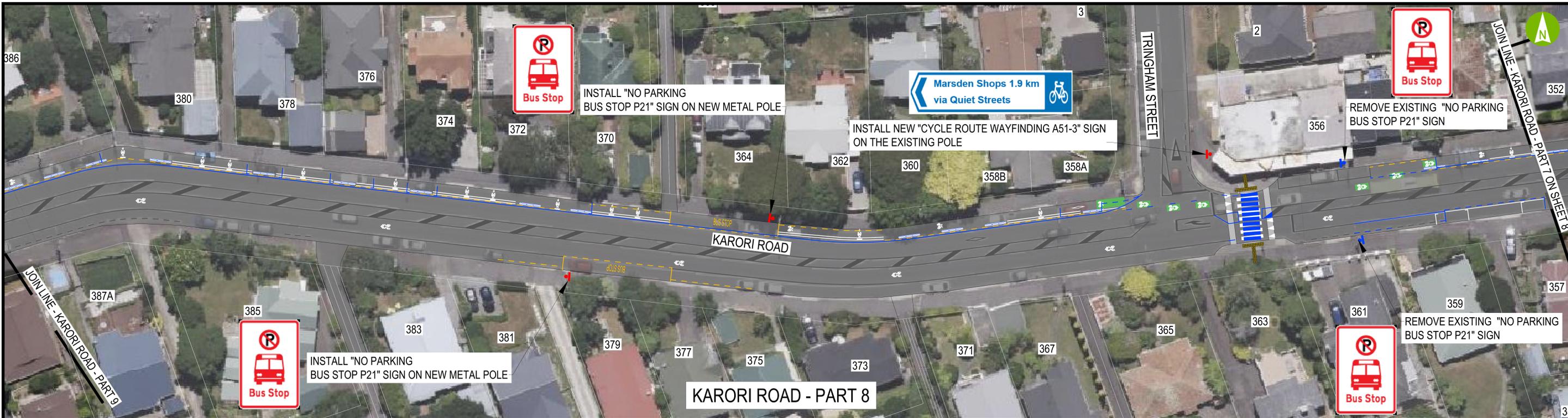
| REV | DATE | REVISION DETAILS | APPROVED | SCALE | SIZE |
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| 0 | 27/03/24 | FOR CONSTRUCTION | G MURISON | 1:250 | A0 |

FOR CONSTRUCTION

| APPROVED | DATE |
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| PROJECT | TITLE |
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| KARORI TO BOTANIC GARDEN KI PAKAKA CONNECTIONS WCC TRANSITIONAL CYCLEWAYS | SIGNAGE PRIMARY ROUTE - IFC DESIGNS KARORI ROAD SHEET 8 (OF 9) |

| DRAWING No. | PROJECT No. | AREA | ITC | DWG | NUMBER | REV |
|-------------|-------------|------|-----|------|--------|-----|
| 522550 | 0000 | DRG | DGN | 0118 | 0 | 0 |



KARORI ROAD - PART 8

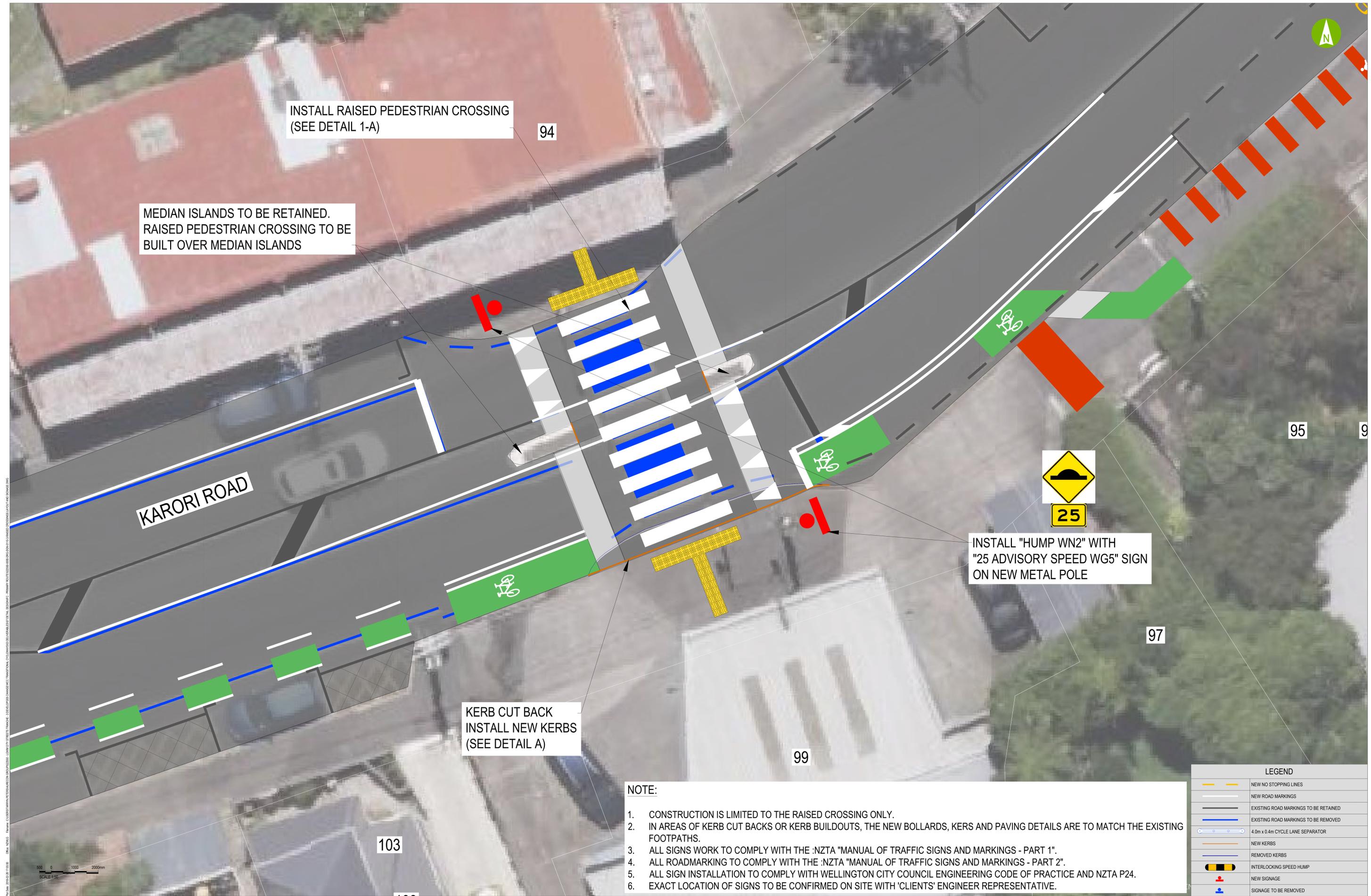


KARORI ROAD - PART 9

NOTE:

1. ALL SIGNS WORK TO COMPLY WITH THE :NZTA "MANUAL OF TRAFFIC SIGNS AND MARKINGS - PART 1".
2. ALL ROADMARKING TO COMPLY WITH THE :NZTA "MANUAL OF TRAFFIC SIGNS AND MARKINGS - PART 2".
3. ALL SIGN INSTALLATION TO COMPLY WITH WELLINGTON CITY COUNCIL ENGINEERING CODE OF PRACTICE AND NZTA P24.
4. EXACT LOCATION OF SIGNS TO BE CONFIRMED ON SITE WITH 'CLIENTS' ENGINEER REPRESENTATIVE.

| LEGEND | |
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| | NEW NO STOPPING LINES |
| | NEW ROAD MARKINGS |
| | EXISTING ROAD MARKINGS TO BE RETAINED |
| | EXISTING ROAD MARKINGS TO BE REMOVED |
| | 4.0m x 0.4m CYCLE LANE SEPARATOR |
| | NEW KERBS |
| | REMOVED KERBS |
| | 50mm HIGH INTERLOCKING SPEED HUMP |
| | NEW SIGNAGE |
| | SIGNAGE TO BE REMOVED |



INSTALL RAISED PEDESTRIAN CROSSING
(SEE DETAIL 1-A)

MEDIAN ISLANDS TO BE RETAINED.
RAISED PEDESTRIAN CROSSING TO BE
BUILT OVER MEDIAN ISLANDS

KARB CUT BACK
INSTALL NEW KERBS
(SEE DETAIL A)

INSTALL "HUMP WN2" WITH
"25 ADVISORY SPEED WG5" SIGN
ON NEW METAL POLE

NOTE:

1. CONSTRUCTION IS LIMITED TO THE RAISED CROSSING ONLY.
2. IN AREAS OF KERB CUT BACKS OR KERB BUILDOUTS, THE NEW BOLLARDS, KERS AND PAVING DETAILS ARE TO MATCH THE EXISTING FOOTPATHS.
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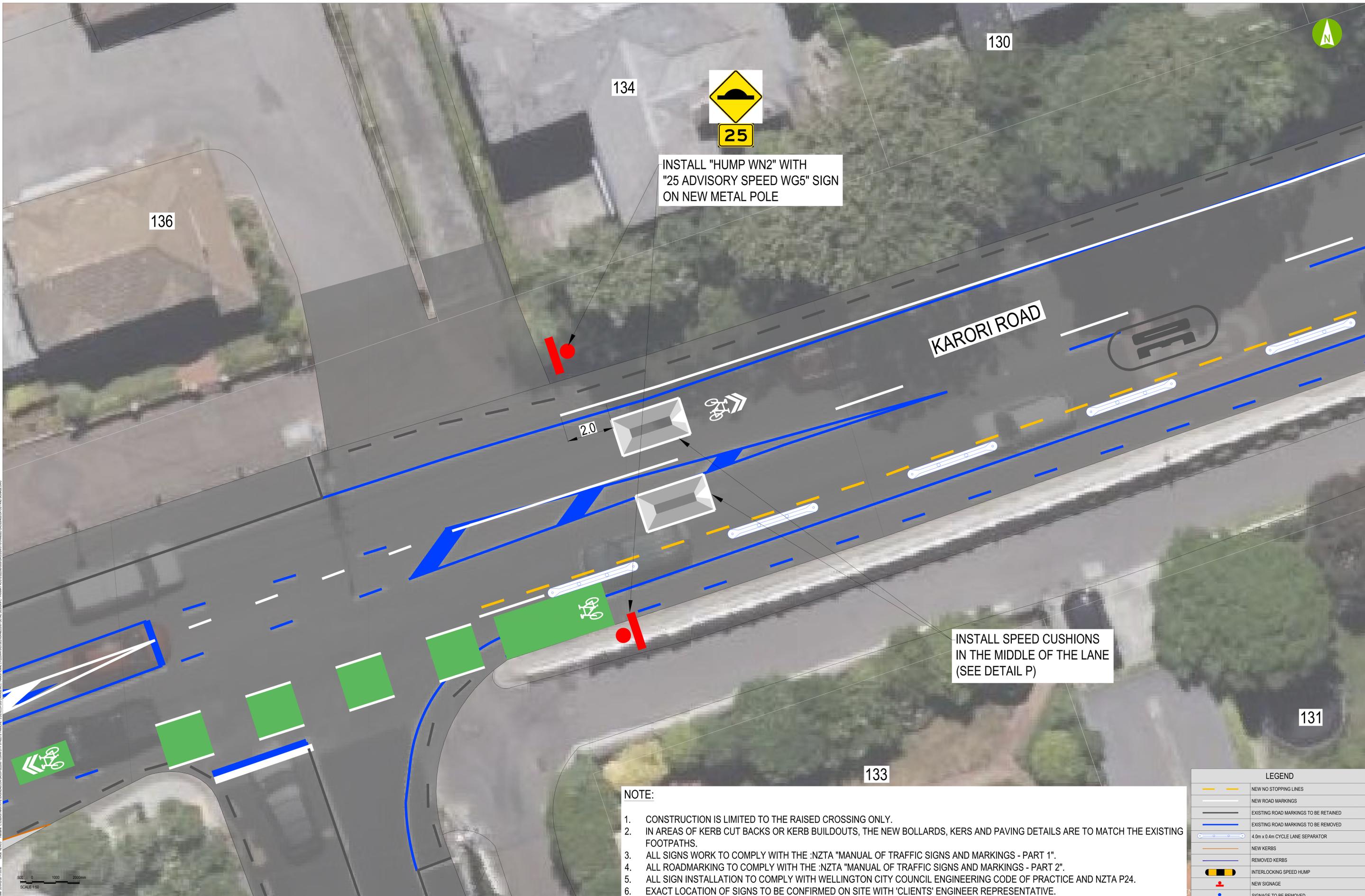
| LEGEND | |
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| | EXISTING ROAD MARKINGS TO BE RETAINED |
| | EXISTING ROAD MARKINGS TO BE REMOVED |
| | 4.0m x 0.4m CYCLE LANE SEPARATOR |
| | NEW KERBS |
| | REMOVED KERBS |
| | INTERLOCKING SPEED HUMP |
| | NEW SIGNAGE |
| | SIGNAGE TO BE REMOVED |

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| REV | DATE | REVISION DETAILS | APPROVED | SCALE | SIZE |
|-----|----------|--|-----------|-------|------|
| 0 | 15/10/23 | FOR CONSTRUCTION | G MURISON | 1:50 | A0 |
| 1 | 26/02/24 | FOR CONSTRUCTION - RAISED PEDESTRIAN CROSSINGS LINEMARKING UPDATED | G MURISON | | |
| 2 | 15/03/24 | FOR CONSTRUCTION - SIGNAGE PLACEMENT NOTE ADDED | G MURISON | | |
| 3 | 27/03/24 | FOR CONSTRUCTION | G MURISON | | |
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| PROJECT | TITLE | PROJECT No. | AREA | ITC | DWG | DGN | NUMBER | REV |
|--|---|-------------|------|-----|-----|------|--------|-----|
| KARORI TO BOTANIC GARDEN KI PAKAKA CONNECTIONS WCC TRANSITIONAL CYCLEWAYS | RAISED CROSSINGS LAYOUT AND SIGNAGE - IFC DESIGNS KARORI ROAD - NEAR NOTTINGHAM STREET SHEET 3 (OF 9) | 522550 | 0000 | DRG | DGN | 1113 | 3 | |



INSTALL "HUMP WN2" WITH
"25 ADVISORY SPEED WG5" SIGN
ON NEW METAL POLE

INSTALL SPEED CUSHIONS
IN THE MIDDLE OF THE LANE
(SEE DETAIL P)

NOTE:

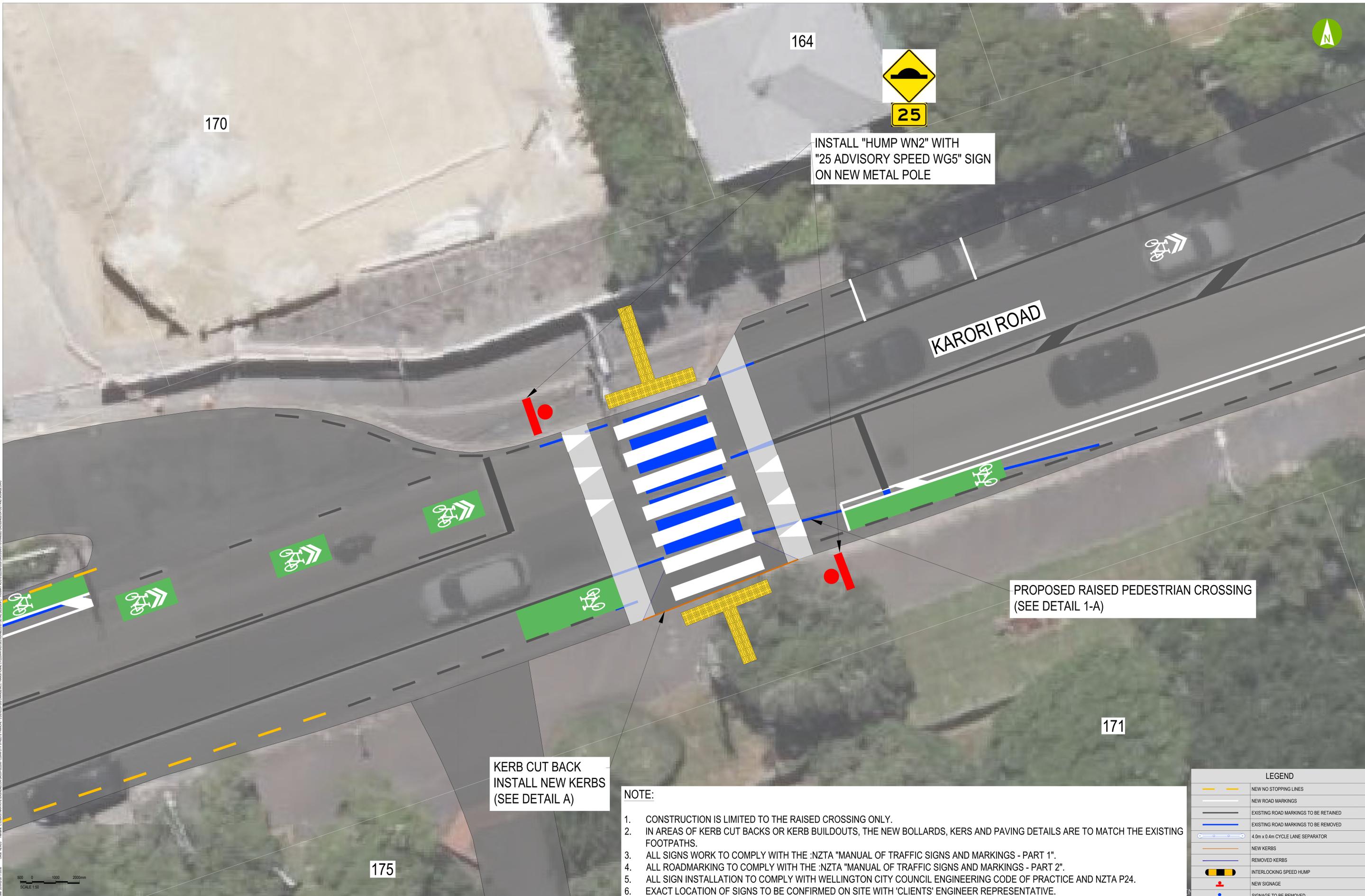
1. CONSTRUCTION IS LIMITED TO THE RAISED CROSSING ONLY.
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5. ALL SIGN INSTALLATION TO COMPLY WITH WELLINGTON CITY COUNCIL ENGINEERING CODE OF PRACTICE AND NZTA P24.
6. EXACT LOCATION OF SIGNS TO BE CONFIRMED ON SITE WITH 'CLIENTS' ENGINEER REPRESENTATIVE.

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| | NEW KERBS |
| | REMOVED KERBS |
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| 0 | 15/10/23 | FOR CONSTRUCTION | G MURISON | 1:50 | A0 |
| 1 | 26/02/24 | FOR CONSTRUCTION - RAISED PEDESTRIAN CROSSINGS LINEMARKING UPDATED | G MURISON | | |
| 2 | 15/03/24 | FOR CONSTRUCTION - SIGNAGE PLACEMENT NOTE ADDED | G MURISON | | |
| 3 | 27/03/24 | FOR CONSTRUCTION | G MURISON | | |

| APPROVED | DATE |
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| DESIGNED | |
| REVIEWED | |
| S HAMILTON | |

| PROJECT | TITLE | DRAWING No. | PROJECT No. | AREA | ITC | DGN | NUMBER | REV |
|--|--|-------------|-------------|------|-----|------|--------|-----|
| KARORI TO BOTANIC GARDEN KI PAKAKA CONNECTIONS WCC TRANSITIONAL CYCLEWAYS | RAISED CROSSINGS LAYOUT AND SIGNAGE - IFC DESIGNS KARORI ROAD - NEAR LANCASTER STREET SHEET 4 (OF 9) | 522550 | 0000 | DRG | DGN | 1114 | 3 | |



INSTALL "HUMP WN2" WITH "25 ADVISORY SPEED WG5" SIGN ON NEW METAL POLE

PROPOSED RAISED PEDESTRIAN CROSSING (SEE DETAIL 1-A)

KERB CUT BACK INSTALL NEW KERBS (SEE DETAIL A)

NOTE:

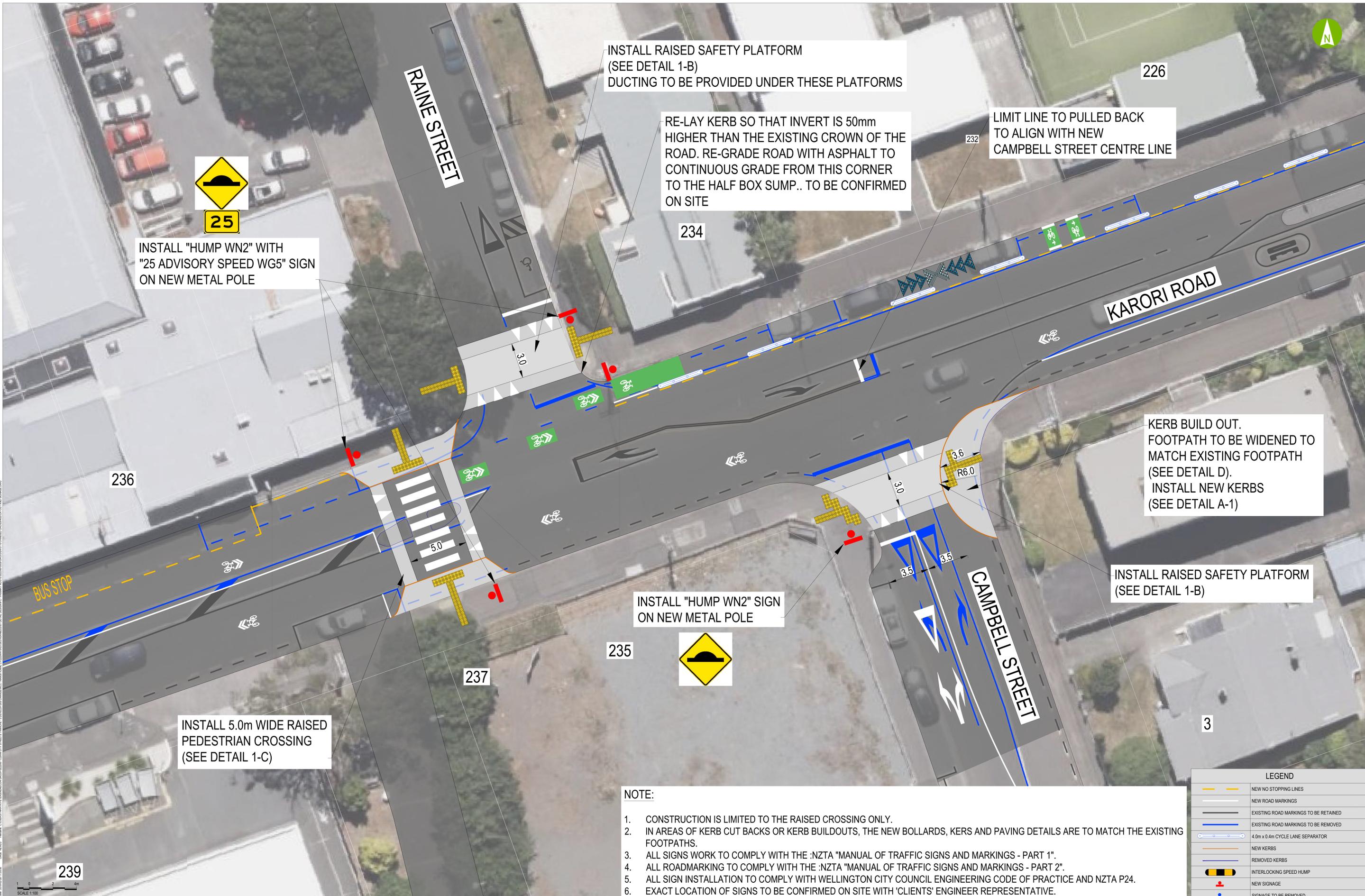
1. CONSTRUCTION IS LIMITED TO THE RAISED CROSSING ONLY.
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| | EXISTING ROAD MARKINGS TO BE REMOVED |
| | 4.0m x 0.4m CYCLE LANE SEPARATOR |
| | NEW KERBS |
| | REMOVED KERBS |
| | INTERLOCKING SPEED HUMP |
| | NEW SIGNAGE |
| | SIGNAGE TO BE REMOVED |



| REV | DATE | REVISION DETAILS | APPROVED | SCALE | SIZE |
|-----|----------|--|-----------|-------|------|
| 0 | 15/10/23 | FOR CONSTRUCTION | G MURISON | 1:50 | A0 |
| 1 | 26/02/24 | FOR CONSTRUCTION - RAISED PEDESTRIAN CROSSINGS LINEMARKING UPDATED | G MURISON | | |
| 2 | 15/03/24 | FOR CONSTRUCTION - SIGNAGE PLACEMENT NOTE ADDED | G MURISON | | |
| 3 | 27/03/24 | FOR CONSTRUCTION | G MURISON | | |

| FOR CONSTRUCTION | PROJECT | TITLE |
|------------------|---|---|
| APPROVED | KARORI TO BOTANIC GARDEN KI PAKAKA CONNECTIONS WCC TRANSITIONAL CYCLEWAYS | RAISED CROSSINGS LAYOUT AND SIGNAGE - IFC DESIGNS KARORI ROAD - NEAR FANCOURT STREET SHEET 5 (OF 9) |
| DATE | PROJECT No. 522550 | DRAWING No. 0000 |
| | AREA 0000 | ITC DRG |
| | DGC DGN | NUMBER 1115 |
| | | REV 3 |



INSTALL "HUMP WN2" WITH "25 ADVISORY SPEED WG5" SIGN ON NEW METAL POLE

INSTALL RAISED SAFETY PLATFORM (SEE DETAIL 1-B)
DUCTING TO BE PROVIDED UNDER THESE PLATFORMS

RE-LAY KERB SO THAT INVERT IS 50mm HIGHER THAN THE EXISTING CROWN OF THE ROAD. RE-GRADE ROAD WITH ASPHALT TO CONTINUOUS GRADE FROM THIS CORNER TO THE HALF BOX SUMP.. TO BE CONFIRMED ON SITE

LIMIT LINE TO PULLED BACK TO ALIGN WITH NEW CAMPBELL STREET CENTRE LINE

KERB BUILD OUT. FOOTPATH TO BE WIDENED TO MATCH EXISTING FOOTPATH (SEE DETAIL D).
INSTALL NEW KERBS (SEE DETAIL A-1)

INSTALL RAISED SAFETY PLATFORM (SEE DETAIL 1-B)

INSTALL "HUMP WN2" SIGN ON NEW METAL POLE



INSTALL 5.0m WIDE RAISED PEDESTRIAN CROSSING (SEE DETAIL 1-C)

NOTE:

- CONSTRUCTION IS LIMITED TO THE RAISED CROSSING ONLY.
- IN AREAS OF KERB CUT BACKS OR KERB BUILDOUTS, THE NEW BOLLARDS, KERS AND PAVING DETAILS ARE TO MATCH THE EXISTING FOOTPATHS.
- ALL SIGNS WORK TO COMPLY WITH THE :NZTA "MANUAL OF TRAFFIC SIGNS AND MARKINGS - PART 1".
- ALL ROADMARKING TO COMPLY WITH THE :NZTA "MANUAL OF TRAFFIC SIGNS AND MARKINGS - PART 2".
- ALL SIGN INSTALLATION TO COMPLY WITH WELLINGTON CITY COUNCIL ENGINEERING CODE OF PRACTICE AND NZTA P24.
- EXACT LOCATION OF SIGNS TO BE CONFIRMED ON SITE WITH 'CLIENTS' ENGINEER REPRESENTATIVE.

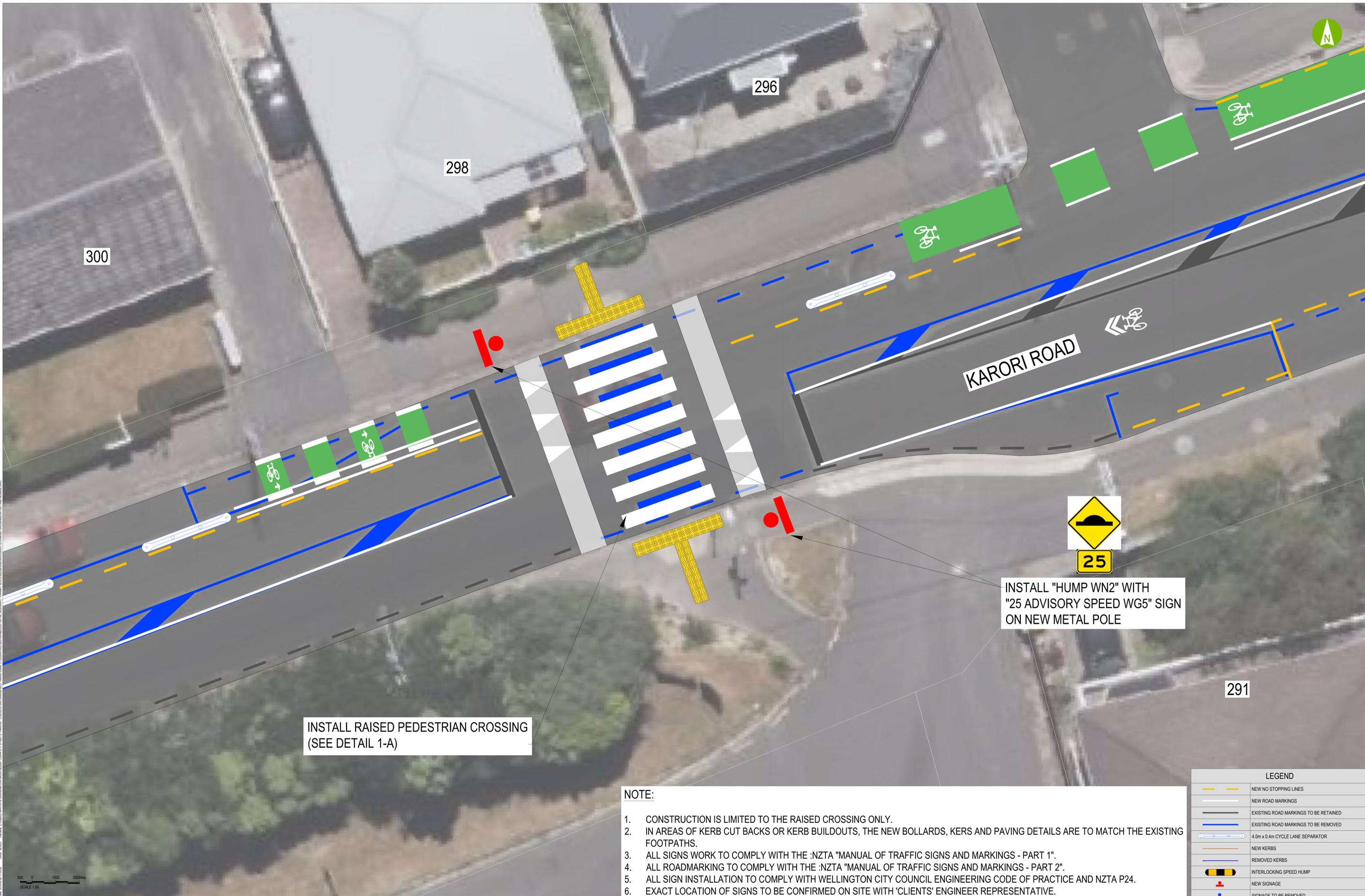
| LEGEND | |
|--------|---------------------------------------|
| | NEW NO STOPPING LINES |
| | NEW ROAD MARKINGS |
| | EXISTING ROAD MARKINGS TO BE RETAINED |
| | EXISTING ROAD MARKINGS TO BE REMOVED |
| | 4.0m x 0.4m CYCLE LANE SEPARATOR |
| | NEW KERBS |
| | REMOVED KERBS |
| | INTERLOCKING SPEED HUMP |
| | NEW SIGNAGE |
| | SIGNAGE TO BE REMOVED |



| REV | DATE | REVISION DETAILS | APPROVED | SCALE | SIZE |
|-----|----------|--|------------|-------|------|
| 0 | 15/10/23 | FOR CONSTRUCTION - RAISED PEDESTRIAN CROSSINGS LINEMARKING UPDATED | G MURISON | 1:100 | A0 |
| 1 | 26/02/24 | FOR CONSTRUCTION - SIGNAGE PLACEMENT NOTE ADDED | G MURISON | | |
| 2 | 15/03/24 | FOR CONSTRUCTION - SIGNAGE PLACEMENT NOTE ADDED | G MURISON | | |
| 3 | 27/03/24 | FOR CONSTRUCTION - NOTE ADDED FOR KERB RE-LAYING | G MURISON | | |
| | | | T PETERS | | |
| | | | T PETERS | | |
| | | | S HAMILTON | | |

| FOR CONSTRUCTION | APPROVED | DATE |
|------------------|----------|------|
| | | |

| PROJECT | TITLE | PROJECT No. | AREA | ITC | DCC | NUMBER | REV |
|---|--|-------------|------|-----|-----|--------|-----|
| KARORI TO BOTANIC GARDEN KI PĀEKAKA CONNECTIONS WCC TRANSITIONAL CYCLEWAYS | RAISED CROSSINGS LAYOUT AND SIGNAGE - IFC DESIGNS KARORI ROAD - AT CAMPBELL STREET AND RAINE STREET SHEET 6 (OF 9) | 522550 | 0000 | DRG | DGN | 1116 | 3 |



INSTALL RAISED PEDESTRIAN CROSSING
(SEE DETAIL 1-A)

INSTALL "HUMP WN2" WITH
"25 ADVISORY SPEED WG5" SIGN
ON NEW METAL POLE

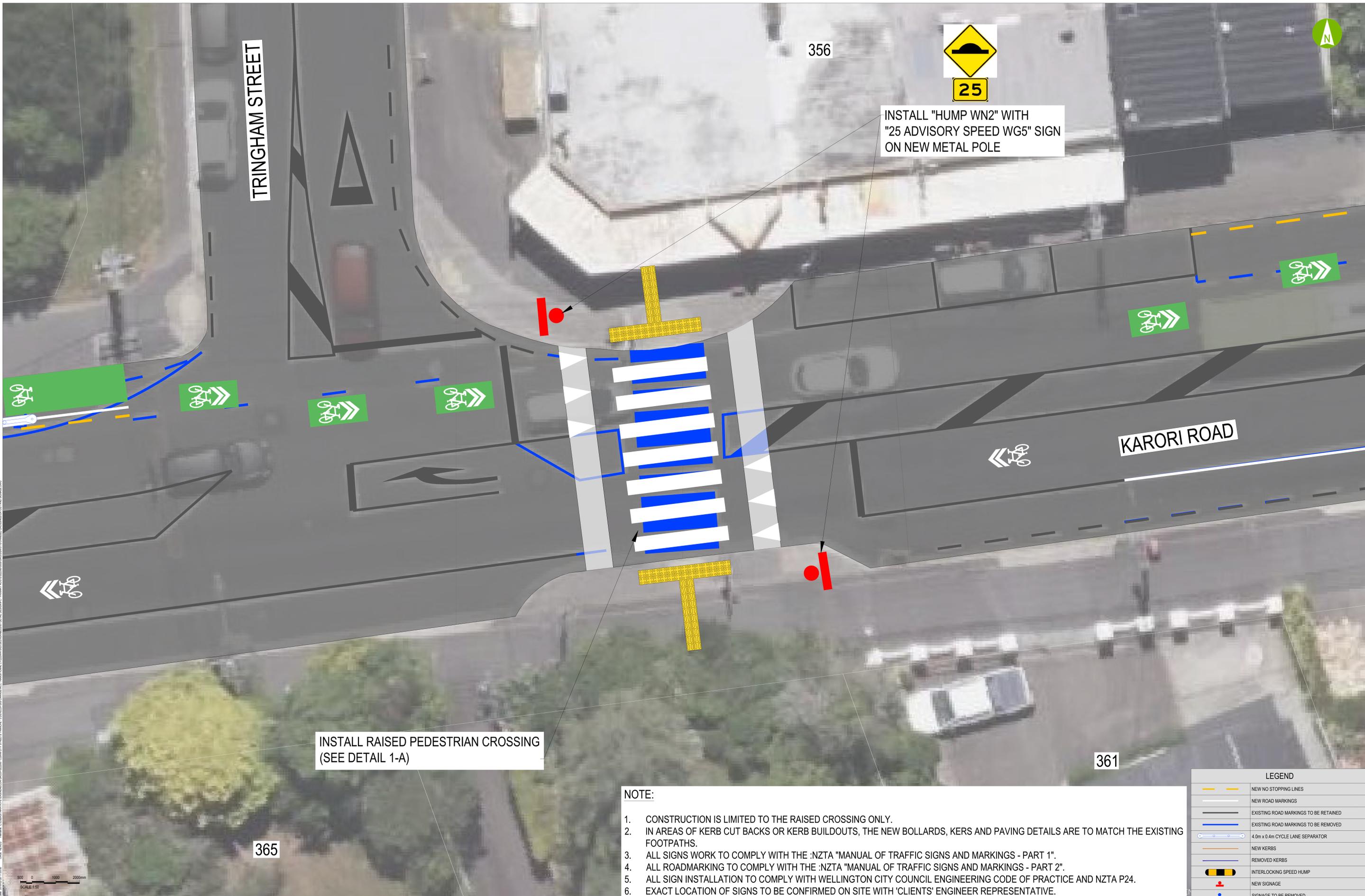
NOTE:

1. CONSTRUCTION IS LIMITED TO THE RAISED CROSSING ONLY.
2. IN AREAS OF KERB CUT BACKS OR KERB BUILDOUTS, THE NEW BOLLARDS, KERS AND PAVING DETAILS ARE TO MATCH THE EXISTING FOOTPATHS.
3. ALL SIGNS WORK TO COMPLY WITH THE :NZTA "MANUAL OF TRAFFIC SIGNS AND MARKINGS - PART 1".
4. ALL ROADMARKING TO COMPLY WITH THE :NZTA "MANUAL OF TRAFFIC SIGNS AND MARKINGS - PART 2".
5. ALL SIGN INSTALLATION TO COMPLY WITH WELLINGTON CITY COUNCIL ENGINEERING CODE OF PRACTICE AND NZTA P24.
6. EXACT LOCATION OF SIGNS TO BE CONFIRMED ON SITE WITH 'CLIENTS' ENGINEER REPRESENTATIVE.

| LEGEND | |
|--------|---------------------------------------|
| | NEW NO STOPPING LINES |
| | NEW ROAD MARKINGS |
| | EXISTING ROAD MARKINGS TO BE RETAINED |
| | EXISTING ROAD MARKINGS TO BE REMOVED |
| | 4.0m x 0.4m CYCLE LANE SEPARATOR |
| | NEW KERBS |
| | REMOVED KERBS |
| | INTERLOCKING SPEED HUMP |
| | NEW SIGNAGE |
| | SIGNAGE TO BE REMOVED |

| REV | DATE | REVISION DETAILS | APPROVED | SCALE | SIZE |
|-----|----------|--|-----------|-------|------|
| 0 | 15/10/23 | FOR CONSTRUCTION | G MURISON | 1:50 | A0 |
| 1 | 26/02/24 | FOR CONSTRUCTION - RAISED PEDESTRIAN CROSSINGS LINEMARKING UPDATED | G MURISON | | |
| 2 | 15/03/24 | FOR CONSTRUCTION - SIGNAGE PLACEMENT NOTE ADDED | G MURISON | | |
| 3 | 27/03/24 | FOR CONSTRUCTION | G MURISON | | |
| | | | | | |
| | | | | | |
| | | | | | |
| | | | | | |

| PROJECT | TITLE | PROJECT No. | AREA | ITC | DGC | NUMBER | REV |
|---|--|-------------|------|-----|-----|--------|-----|
| KARORI TO BOTANIC GARDEN KI PĀEKAKA CONNECTIONS WCC TRANSITIONAL CYCLEWAYS | RAISED CROSSINGS LAYOUT AND SIGNAGE - IFC DESIGNS KARORI ROAD - NEAR EAGLE STREET SHEET 7 (OF 9) | 522550 | 0000 | DRG | DGN | 1117 | 3 |



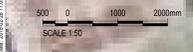
INSTALL "HUMP WN2" WITH "25 ADVISORY SPEED WG5" SIGN ON NEW METAL POLE

INSTALL RAISED PEDESTRIAN CROSSING (SEE DETAIL 1-A)

NOTE:

1. CONSTRUCTION IS LIMITED TO THE RAISED CROSSING ONLY.
2. IN AREAS OF KERB CUT BACKS OR KERB BUILDOUTS, THE NEW BOLLARDS, KERS AND PAVING DETAILS ARE TO MATCH THE EXISTING FOOTPATHS.
3. ALL SIGNS WORK TO COMPLY WITH THE :NZTA "MANUAL OF TRAFFIC SIGNS AND MARKINGS - PART 1".
4. ALL ROADMARKING TO COMPLY WITH THE :NZTA "MANUAL OF TRAFFIC SIGNS AND MARKINGS - PART 2".
5. ALL SIGN INSTALLATION TO COMPLY WITH WELLINGTON CITY COUNCIL ENGINEERING CODE OF PRACTICE AND NZTA P24.
6. EXACT LOCATION OF SIGNS TO BE CONFIRMED ON SITE WITH 'CLIENTS' ENGINEER REPRESENTATIVE.

| LEGEND | |
|--------|---------------------------------------|
| | NEW NO STOPPING LINES |
| | NEW ROAD MARKINGS |
| | EXISTING ROAD MARKINGS TO BE RETAINED |
| | EXISTING ROAD MARKINGS TO BE REMOVED |
| | 4.0m x 0.4m CYCLE LANE SEPARATOR |
| | NEW KERBS |
| | REMOVED KERBS |
| | INTERLOCKING SPEED HUMPS |
| | NEW SIGNAGE |
| | SIGNAGE TO BE REMOVED |



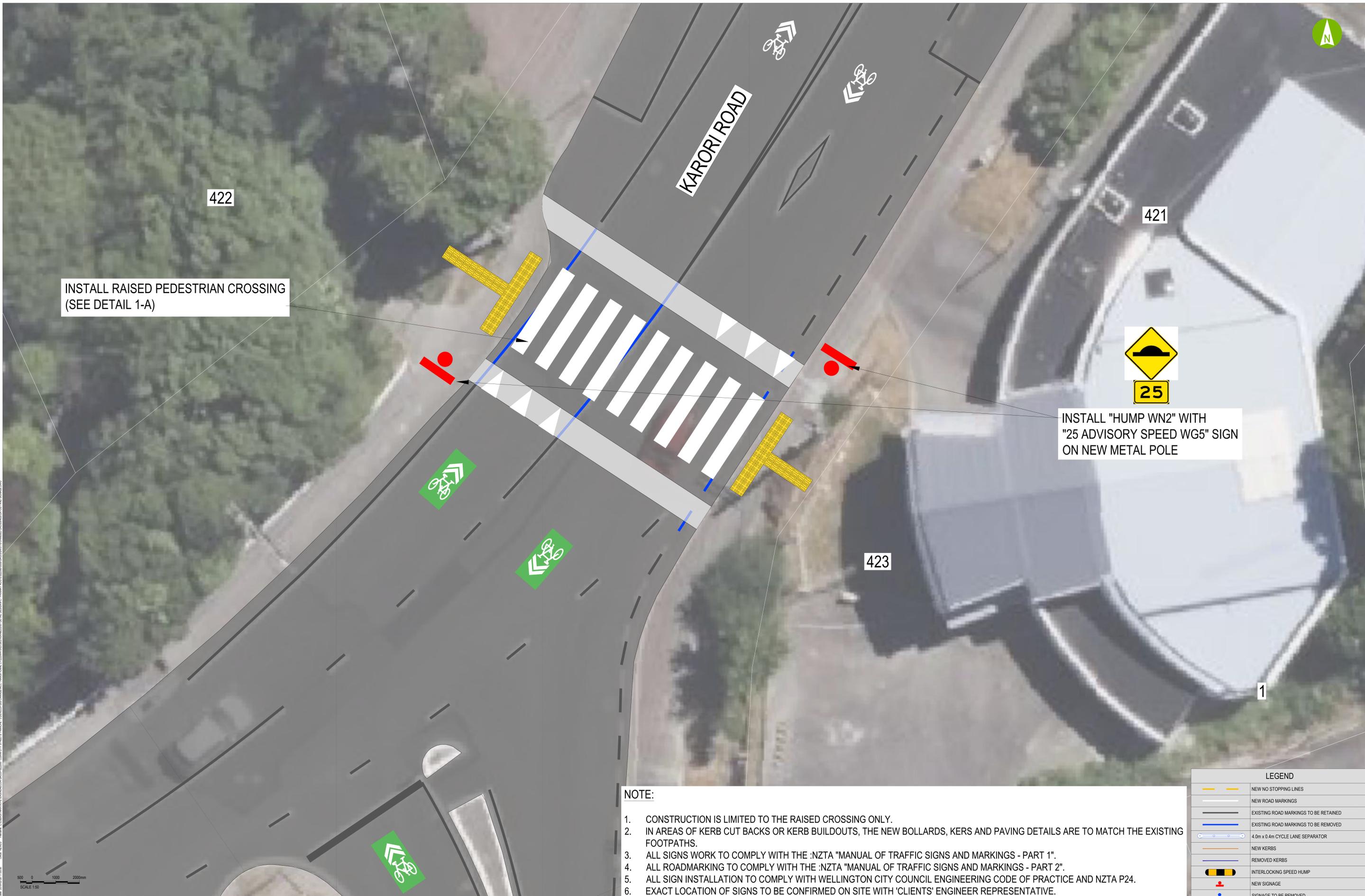
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| REV | DATE | REVISION DETAILS | APPROVED | SCALE | SIZE |
|-----|----------|--|-----------|-------------------|------|
| 0 | 15/10/23 | FOR CONSTRUCTION | G MURISON | 1:50 | A0 |
| 1 | 26/02/24 | FOR CONSTRUCTION - RAISED PEDESTRIAN CROSSINGS LINEMARKING UPDATED | G MURISON | | |
| 2 | 15/03/24 | FOR CONSTRUCTION - SIGNAGE PLACEMENT NOTE ADDED | G MURISON | | |
| 3 | 27/03/24 | FOR CONSTRUCTION | G MURISON | | |
| | | | | DRAWN | |
| | | | | T PETERS | |
| | | | | DESIGNED | |
| | | | | T PETERS | |
| | | | | REVIEWED | |
| | | | | S HAMILTON | |

| FOR CONSTRUCTION | APPROVED | DATE |
|------------------|----------|------|
| | | |

| PROJECT | TITLE | DRAWING No. | PROJECT No. | AREA | ITC | DRG | DGN | NUMBER | REV |
|--|---|-------------|-------------|------|-----|-----|-----|--------|-----|
| KARORI TO BOTANIC GARDEN KI PĀEKAKA CONNECTIONS WCC TRANSITIONAL CYCLEWAYS | RAISED CROSSINGS LAYOUT AND SIGNAGE - IFC DESIGNS KARORI ROAD - NEAR TRINGHAM STREET SHEET 8 (OF 9) | 522550 | 0000 | | | | | 1118 | 3 |



INSTALL RAISED PEDESTRIAN CROSSING
(SEE DETAIL 1-A)

INSTALL "HUMP WN2" WITH
"25 ADVISORY SPEED WG5" SIGN
ON NEW METAL POLE

NOTE:

1. CONSTRUCTION IS LIMITED TO THE RAISED CROSSING ONLY.
2. IN AREAS OF KERB CUT BACKS OR KERB BUILDOUTS, THE NEW BOLLARDS, KERS AND PAVING DETAILS ARE TO MATCH THE EXISTING FOOTPATHS.
3. ALL SIGNS WORK TO COMPLY WITH THE :NZTA "MANUAL OF TRAFFIC SIGNS AND MARKINGS - PART 1".
4. ALL ROADMARKING TO COMPLY WITH THE :NZTA "MANUAL OF TRAFFIC SIGNS AND MARKINGS - PART 2".
5. ALL SIGN INSTALLATION TO COMPLY WITH WELLINGTON CITY COUNCIL ENGINEERING CODE OF PRACTICE AND NZTA P24.
6. EXACT LOCATION OF SIGNS TO BE CONFIRMED ON SITE WITH 'CLIENTS' ENGINEER REPRESENTATIVE.

| LEGEND | |
|--------|---------------------------------------|
| | NEW NO STOPPING LINES |
| | NEW ROAD MARKINGS |
| | EXISTING ROAD MARKINGS TO BE RETAINED |
| | EXISTING ROAD MARKINGS TO BE REMOVED |
| | 4.0m x 0.4m CYCLE LANE SEPARATOR |
| | NEW KERBS |
| | REMOVED KERBS |
| | INTERLOCKING SPEED HUMP |
| | NEW SIGNAGE |
| | SIGNAGE TO BE REMOVED |

SCALE 1:50
500 0 1000 2000mm



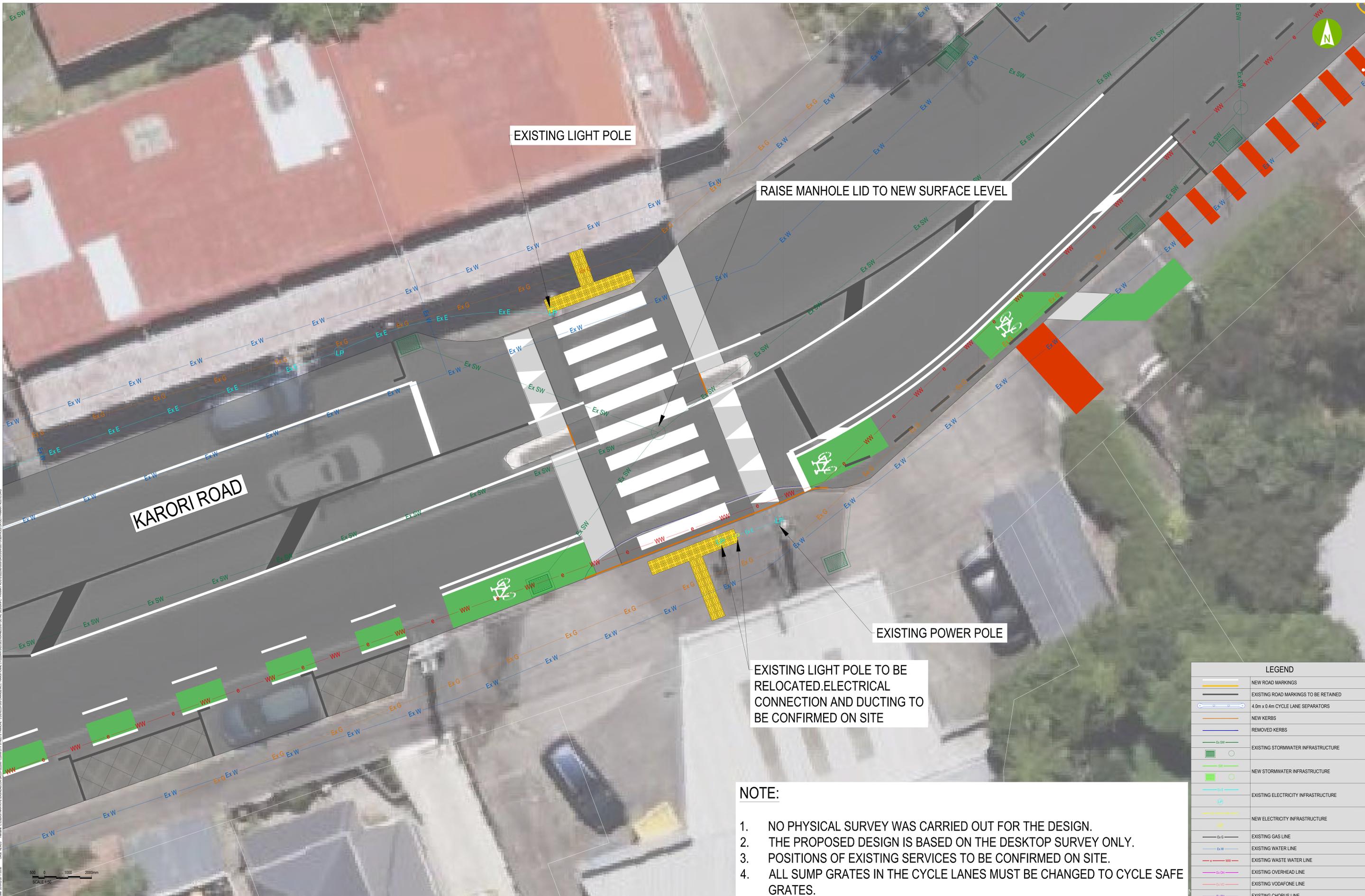
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Wellington City Council**
Me Heke Ki Pōneke

| REV | DATE | REVISION DETAILS | APPROVED | SCALE | SIZE |
|-----|----------|--|-----------|------------------------|------|
| 0 | 15/10/23 | FOR CONSTRUCTION | G MURISON | 1:50 | A0 |
| 1 | 26/02/24 | FOR CONSTRUCTION - RAISED PEDESTRIAN CROSSINGS LINEMARKING UPDATED | G MURISON | | |
| 2 | 15/03/24 | FOR CONSTRUCTION - SIGNAGE PLACEMENT NOTE ADDED | G MURISON | | |
| 3 | 27/03/24 | FOR CONSTRUCTION | G MURISON | | |
| | | | | DRAWN T PETERS | |
| | | | | DESIGNED T PETERS | |
| | | | | REVIEWED S HAMILTON | |

| FOR CONSTRUCTION | APPROVED | DATE |
|------------------|----------|------|
| | | |

| PROJECT | TITLE | PROJECT No. | AREA | ITC | DGC | NUMBER | REV |
|---|---|-------------|------|-----|-----|--------|-----|
| KARORI TO BOTANIC GARDEN KI PĀEKAKA CONNECTIONS WCC TRANSITIONAL CYCLEWAYS | RAISED CROSSINGS LAYOUT AND SIGNAGE - IFC DESIGNS KARORI ROAD - NEAR TRINGHAM STREET SHEET 9 (OF 9) | 522550 | 0000 | DRG | DGN | 1119 | 3 |



KARORI ROAD

EXISTING LIGHT POLE

RAISE MANHOLE LID TO NEW SURFACE LEVEL

EXISTING POWER POLE

EXISTING LIGHT POLE TO BE RELOCATED. ELECTRICAL CONNECTION AND DUCTING TO BE CONFIRMED ON SITE

NOTE:

1. NO PHYSICAL SURVEY WAS CARRIED OUT FOR THE DESIGN.
2. THE PROPOSED DESIGN IS BASED ON THE DESKTOP SURVEY ONLY.
3. POSITIONS OF EXISTING SERVICES TO BE CONFIRMED ON SITE.
4. ALL SUMP GRATES IN THE CYCLE LANES MUST BE CHANGED TO CYCLE SAFE GRATES.

| LEGEND | |
|--------|---------------------------------------|
| | NEW ROAD MARKINGS |
| | EXISTING ROAD MARKINGS TO BE RETAINED |
| | 4.0m x 0.4m CYCLE LANE SEPARATORS |
| | NEW KERBS |
| | REMOVED KERBS |
| | EXISTING STORMWATER INFRASTRUCTURE |
| | NEW STORMWATER INFRASTRUCTURE |
| | EXISTING ELECTRICITY INFRASTRUCTURE |
| | NEW ELECTRICITY INFRASTRUCTURE |
| | EXISTING GAS LINE |
| | EXISTING WATER LINE |
| | EXISTING WASTE WATER LINE |
| | EXISTING OVERHEAD LINE |
| | EXISTING VODAFONE LINE |
| | EXISTING CHORUS LINE |



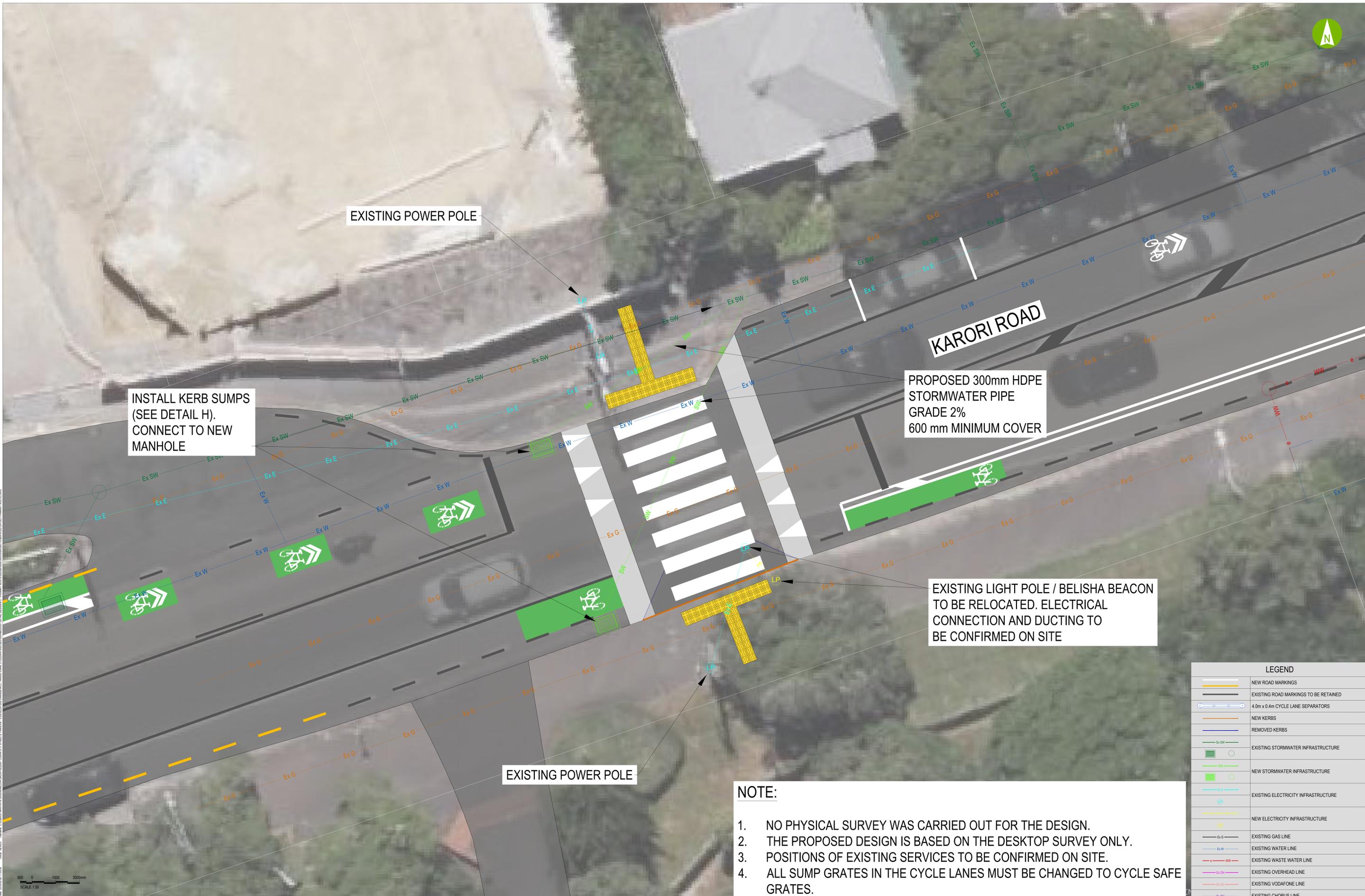
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| REV | DATE | REVISION DETAILS | APPROVED | SCALE | SIZE |
|-----|----------|------------------|-----------|-------|------|
| 0 | 15/12/23 | FOR CONSTRUCTION | G MURISON | 1:50 | A0 |
| 1 | 27/03/24 | FOR CONSTRUCTION | G MURISON | | |

| APPROVED | DATE |
|----------|------|
| | |

| PROJECT | TITLE | DRAWING No. | PROJECT No. | AREA | DATE | NUMBER | REV | |
|---|---|-------------|-------------|------|------|--------|------|---|
| KARORI TO BOTANIC GARDEN KI PAKAKA CONNECTIONS WCC TRANSITIONAL CYCLEWAYS | SERVICE LAYOUT PRIMARY ROUTE - IFC DESIGNS KARORI ROAD - NEAR NOTTINGHAM STREET SHEET 5 (OF 10) | | 522550 | 0000 | DRG | DGN | 0315 | 1 |



NOTE:

- NO PHYSICAL SURVEY WAS CARRIED OUT FOR THE DESIGN.
- THE PROPOSED DESIGN IS BASED ON THE DESKTOP SURVEY ONLY.
- POSITIONS OF EXISTING SERVICES TO BE CONFIRMED ON SITE.
- ALL SUMP GRATES IN THE CYCLE LANES MUST BE CHANGED TO CYCLE SAFE GRATES.

| LEGEND | |
|--------|---------------------------------------|
| | NEW ROAD MARKINGS |
| | EXISTING ROAD MARKINGS TO BE RETAINED |
| | 4.0m x 0.4m CYCLE LANE SEPARATORS |
| | NEW KERBS |
| | REMOVED KERBS |
| | EXISTING STORMWATER INFRASTRUCTURE |
| | NEW STORMWATER INFRASTRUCTURE |
| | EXISTING ELECTRICITY INFRASTRUCTURE |
| | NEW ELECTRICITY INFRASTRUCTURE |
| | EXISTING GAS LINE |
| | EXISTING WATER LINE |
| | EXISTING WASTE WATER LINE |
| | EXISTING OVERHEAD LINE |
| | EXISTING VODAFONE LINE |
| | EXISTING CHORUS LINE |

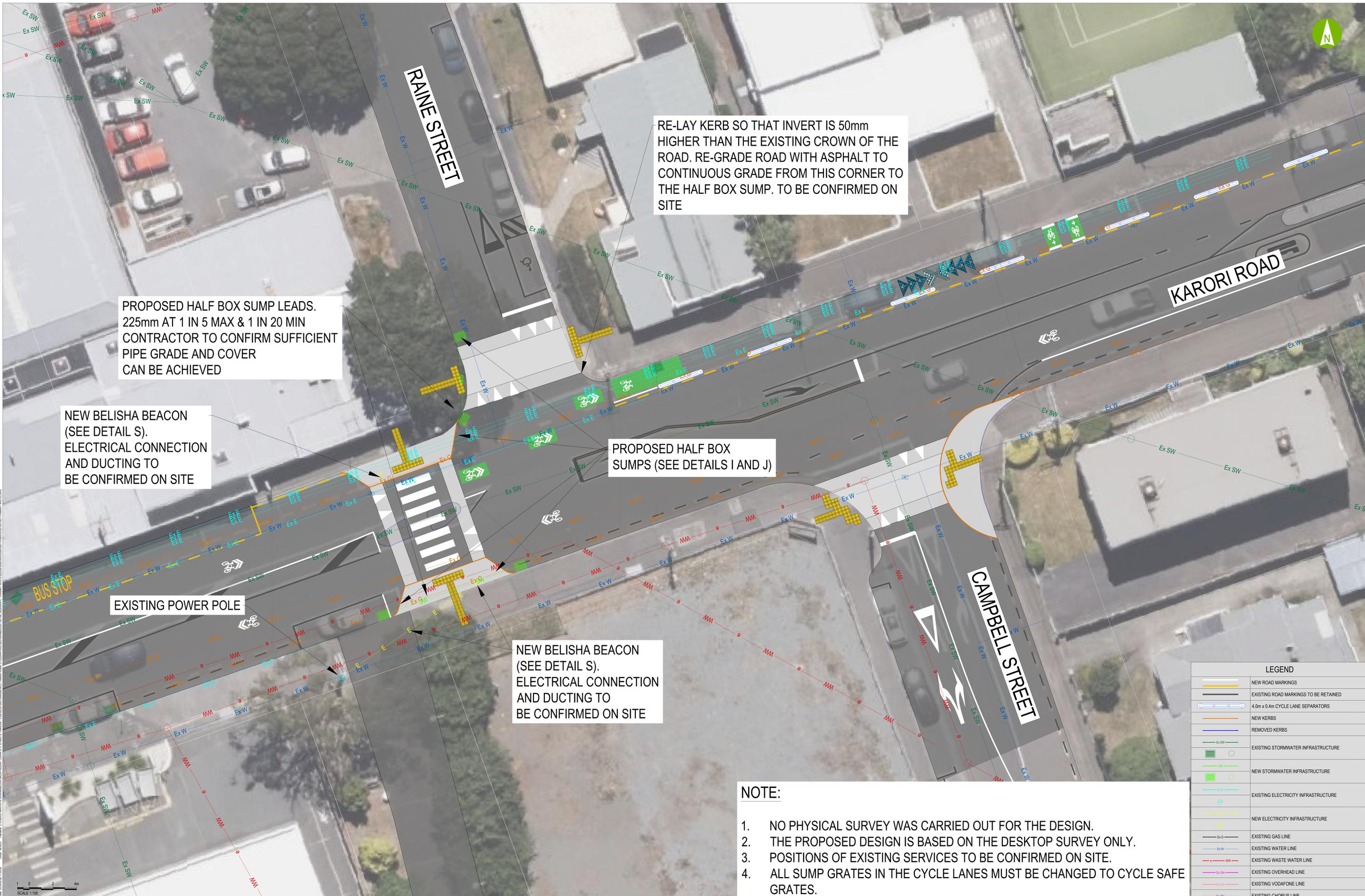
500 0 1000 2000mm
SCALE 1:50



| REV | DATE | REVISION DETAILS | APPROVED | SCALE | SIZE | FOR CONSTRUCTION |
|-----|----------|------------------|-----------|-------|------|------------------|
| 0 | 15/12/23 | FOR CONSTRUCTION | G MURISON | 1:50 | A0 | FOR CONSTRUCTION |
| 1 | 27/03/24 | FOR CONSTRUCTION | G MURISON | | | |

| APPROVED | SCALE | SIZE | FOR CONSTRUCTION |
|--------------|-------|------|------------------|
| G MURISON | 1:50 | A0 | FOR CONSTRUCTION |
| A IBRAHIM | | | |
| A IBRAHIM | | | |
| R VAN LIEROP | | | |

| PROJECT | TITLE | PROJECT No. | AREA | ITC | DGS | NUMBER | REV |
|---|---|-------------|------|-----|-----|--------|-----|
| KARORI TO BOTANIC GARDEN KI PAKAKA CONNECTIONS WCC TRANSITIONAL CYCLEWAYS | SERVICE LAYOUT PRIMARY ROUTE - IFC DESIGNS KARORI ROAD - NEAR FANCOURT STREET SHEET 6 (OF 10) | 522550 | 0000 | DRG | DGN | 0316 | 1 |



RE-LAY KERB SO THAT INVERT IS 50mm HIGHER THAN THE EXISTING CROWN OF THE ROAD. RE-GRADE ROAD WITH ASPHALT TO CONTINUOUS GRADE FROM THIS CORNER TO THE HALF BOX SUMP. TO BE CONFIRMED ON SITE

PROPOSED HALF BOX SUMP LEADS. 225mm AT 1 IN 5 MAX & 1 IN 20 MIN CONTRACTOR TO CONFIRM SUFFICIENT PIPE GRADE AND COVER CAN BE ACHIEVED

NEW BELISHA BEACON (SEE DETAIL S). ELECTRICAL CONNECTION AND DUCTING TO BE CONFIRMED ON SITE

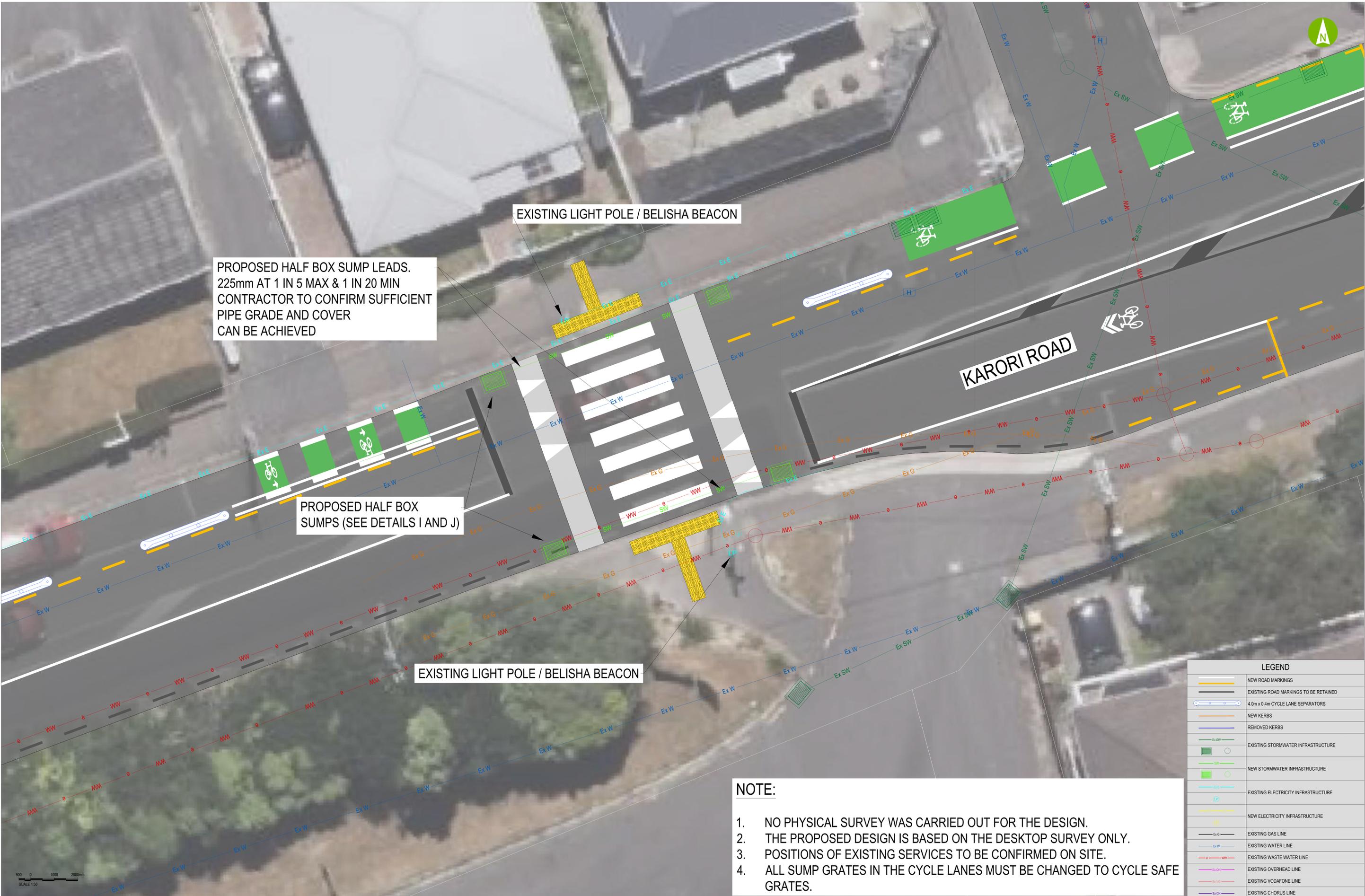
PROPOSED HALF BOX SUMPS (SEE DETAILS I AND J)

EXISTING POWER POLE

NEW BELISHA BEACON (SEE DETAIL S). ELECTRICAL CONNECTION AND DUCTING TO BE CONFIRMED ON SITE

- NOTE:**
- NO PHYSICAL SURVEY WAS CARRIED OUT FOR THE DESIGN.
 - THE PROPOSED DESIGN IS BASED ON THE DESKTOP SURVEY ONLY.
 - POSITIONS OF EXISTING SERVICES TO BE CONFIRMED ON SITE.
 - ALL SUMP GRATES IN THE CYCLE LANES MUST BE CHANGED TO CYCLE SAFE GRATES.

| LEGEND | |
|--------|---------------------------------------|
| | NEW ROAD MARKINGS |
| | EXISTING ROAD MARKINGS TO BE RETAINED |
| | 4.0m x 0.4m CYCLE LANE SEPARATORS |
| | NEW KERBS |
| | REMOVED KERBS |
| | EXISTING STORMWATER INFRASTRUCTURE |
| | NEW STORMWATER INFRASTRUCTURE |
| | EXISTING ELECTRICITY INFRASTRUCTURE |
| | NEW ELECTRICITY INFRASTRUCTURE |
| | EXISTING GAS LINE |
| | EXISTING WATER LINE |
| | EXISTING WASTE WATER LINE |
| | EXISTING OVERHEAD LINE |
| | EXISTING VODAFONE LINE |
| | EXISTING CHORUS LINE |



EXISTING LIGHT POLE / BELISHA BEACON

PROPOSED HALF BOX SUMP LEADS.
225mm AT 1 IN 5 MAX & 1 IN 20 MIN
CONTRACTOR TO CONFIRM SUFFICIENT
PIPE GRADE AND COVER
CAN BE ACHIEVED

PROPOSED HALF BOX
SUMPS (SEE DETAILS I AND J)

EXISTING LIGHT POLE / BELISHA BEACON

KARORI ROAD

NOTE:

1. NO PHYSICAL SURVEY WAS CARRIED OUT FOR THE DESIGN.
2. THE PROPOSED DESIGN IS BASED ON THE DESKTOP SURVEY ONLY.
3. POSITIONS OF EXISTING SERVICES TO BE CONFIRMED ON SITE.
4. ALL SUMP GRATES IN THE CYCLE LANES MUST BE CHANGED TO CYCLE SAFE GRATES.

| LEGEND | |
|--------|---------------------------------------|
| | NEW ROAD MARKINGS |
| | EXISTING ROAD MARKINGS TO BE RETAINED |
| | 4.0m x 0.4m CYCLE LANE SEPARATORS |
| | NEW KERBS |
| | REMOVED KERBS |
| | EXISTING STORMWATER INFRASTRUCTURE |
| | NEW STORMWATER INFRASTRUCTURE |
| | EXISTING ELECTRICITY INFRASTRUCTURE |
| | NEW ELECTRICITY INFRASTRUCTURE |
| | EXISTING GAS LINE |
| | EXISTING WATER LINE |
| | EXISTING WASTE WATER LINE |
| | EXISTING OVERHEAD LINE |
| | EXISTING VODAFONE LINE |
| | EXISTING CHORUS LINE |

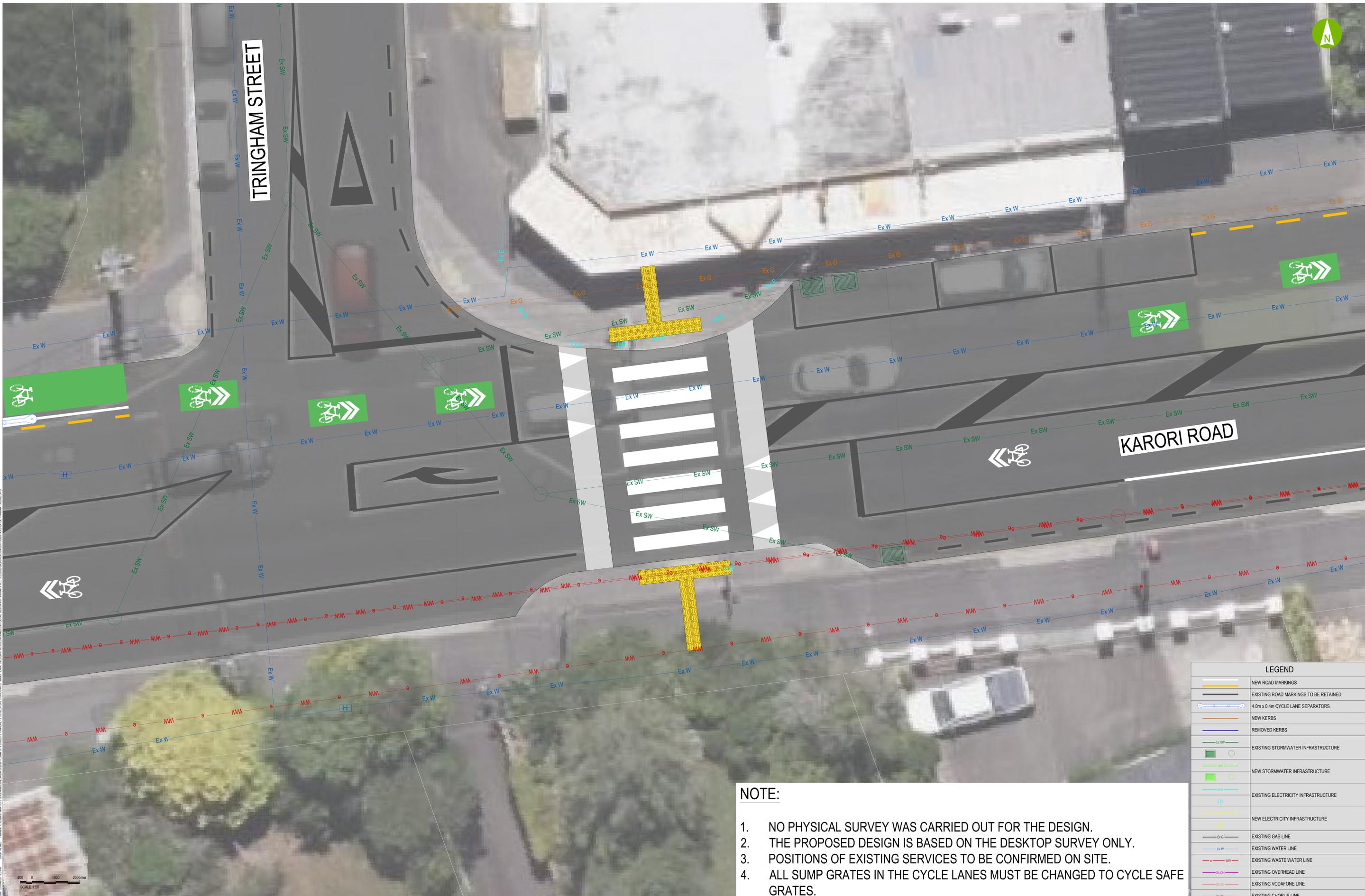


| REV | DATE | REVISION DETAILS | APPROVED |
|-----|----------|------------------|-----------|
| 0 | 15/12/23 | FOR CONSTRUCTION | G MURISON |
| 1 | 27/03/24 | FOR CONSTRUCTION | G MURISON |

| SCALE | SIZE |
|-------|------|
| 1:50 | A0 |

| FOR CONSTRUCTION | APPROVED | DATE |
|------------------|----------|------|
| | | |

| PROJECT | TITLE | DRAWING No. | PROJECT No. | AREA | ITIC | DATE | NUMBER | REV |
|--|--|-------------|-------------|------|------|------|--------|-----|
| KARORI TO BOTANIC GARDEN KI PAKAKA CONNECTIONS WCC TRANSITIONAL CYCLEWAYS | SERVICE LAYOUT PRIMARY ROUTE - IFC DESIGNS KARORI ROAD - NEAR EAGLE STREET SHEET 8 (OF 10) | | 522550 | 0000 | DRG | DGN | 0318 | 1 |



Project: 2019/02/28 10:08:59 Date: 15/12/23 Drawing: C:\Users\peter\OneDrive\Documents\Projects\Karori to Botanic Garden Ki Paekaka Connections\WCC Transitional Cycleways\Primary Route\DRG - Karori Road - Near Tringham Street.dwg

500 0 1000 2000mm
SCALE 1:50

NOTE:

1. NO PHYSICAL SURVEY WAS CARRIED OUT FOR THE DESIGN.
2. THE PROPOSED DESIGN IS BASED ON THE DESKTOP SURVEY ONLY.
3. POSITIONS OF EXISTING SERVICES TO BE CONFIRMED ON SITE.
4. ALL SUMP GRATES IN THE CYCLE LANES MUST BE CHANGED TO CYCLE SAFE GRATES.

| LEGEND | |
|--------|---------------------------------------|
| | NEW ROAD MARKINGS |
| | EXISTING ROAD MARKINGS TO BE RETAINED |
| | 4.0m x 0.4m CYCLE LANE SEPARATORS |
| | NEW KERBS |
| | REMOVED KERBS |
| | EXISTING STORMWATER INFRASTRUCTURE |
| | NEW STORMWATER INFRASTRUCTURE |
| | EXISTING ELECTRICITY INFRASTRUCTURE |
| | NEW ELECTRICITY INFRASTRUCTURE |
| | EXISTING GAS LINE |
| | EXISTING WATER LINE |
| | EXISTING WASTE WATER LINE |
| | EXISTING OVERHEAD LINE |
| | EXISTING VODAFONE LINE |
| | EXISTING CHORUS LINE |



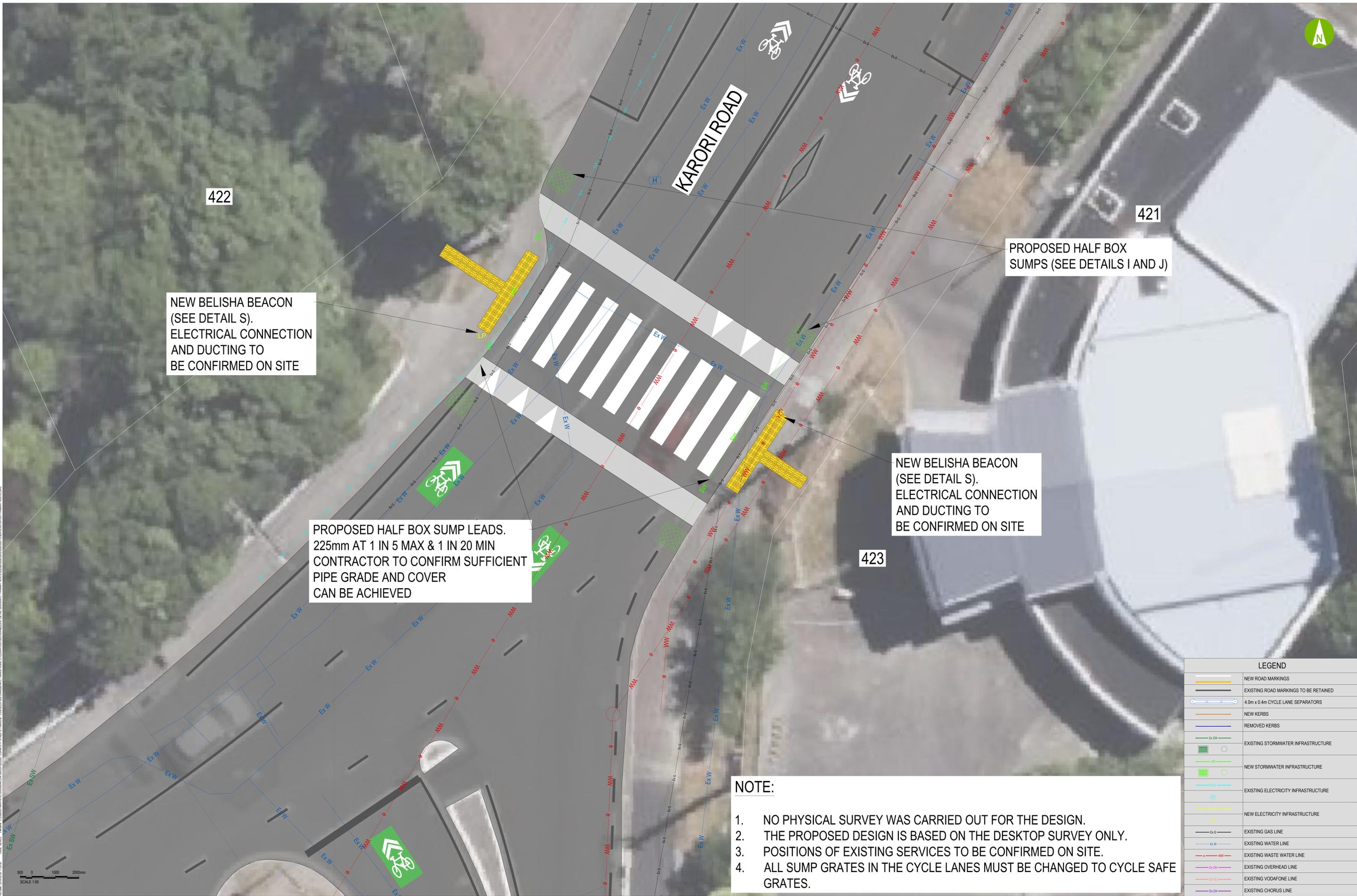
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| REV | DATE | REVISION DETAILS | APPROVED | SCALE | SIZE |
|-----|----------|------------------|-----------|-------------------------|------|
| 0 | 15/12/23 | FOR CONSTRUCTION | G MURISON | 1:50 | A0 |
| 1 | 27/03/24 | FOR CONSTRUCTION | G MURISON | | |
| | | | | DRAWN | |
| | | | | DESIGNED | |
| | | | | REVIEWED | |
| | | | | DATE | |
| | | | | APPROVED | |
| | | | | DATE | |
| | | | | FOR CONSTRUCTION | |

| PROJECT | TITLE |
|---|---|
| KARORI TO BOTANIC GARDEN KI PAEKAKA CONNECTIONS WCC TRANSITIONAL CYCLEWAYS | SERVICE LAYOUT PRIMARY ROUTE - IFC DESIGNS KARORI ROAD - NEAR TRINGHAM STREET SHEET 9 (OF 10) |

| DRAWING No. | PROJECT No. | AREA | ITC | DGS | NUMBER | REV |
|-------------|-------------|------|-----|------|--------|-----|
| 522550 | 0000 | DRG | DGN | 0319 | 1 | |



PROPOSED HALF BOX SUMP LEADS.
225mm AT 1 IN 5 MAX & 1 IN 20 MIN
CONTRACTOR TO CONFIRM SUFFICIENT
PIPE GRADE AND COVER
CAN BE ACHIEVED

NEW BELISHA BEACON
(SEE DETAIL S).
ELECTRICAL CONNECTION
AND DUCTING TO
BE CONFIRMED ON SITE

PROPOSED HALF BOX
SUMPS (SEE DETAILS I AND J)

NOTE:

1. NO PHYSICAL SURVEY WAS CARRIED OUT FOR THE DESIGN.
2. THE PROPOSED DESIGN IS BASED ON THE DESKTOP SURVEY ONLY.
3. POSITIONS OF EXISTING SERVICES TO BE CONFIRMED ON SITE.
4. ALL SUMP GRATES IN THE CYCLE LANES MUST BE CHANGED TO CYCLE SAFE GRATES.

| LEGEND | |
|--------|---------------------------------------|
| | NEW ROAD MARKINGS |
| | EXISTING ROAD MARKINGS TO BE RETAINED |
| | 4.0m x 0.4m CYCLE LANE SEPARATORS |
| | NEW KERBS |
| | REMOVED KERBS |
| | EXISTING STORMWATER INFRASTRUCTURE |
| | NEW STORMWATER INFRASTRUCTURE |
| | EXISTING ELECTRICITY INFRASTRUCTURE |
| | NEW ELECTRICITY INFRASTRUCTURE |
| | EXISTING GAS LINE |
| | EXISTING WATER LINE |
| | EXISTING WASTE WATER LINE |
| | EXISTING OVERHEAD LINE |
| | EXISTING VODAFONE LINE |
| | EXISTING CHORUS LINE |

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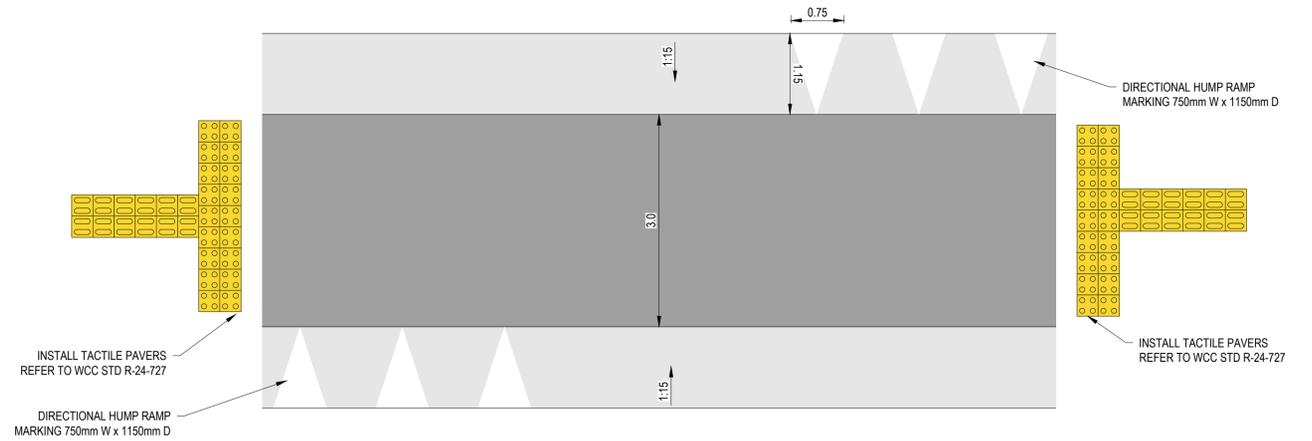
CLIENT
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Wellington City Council**
Me Heke Ki Pōneke

| REV | DATE | REVISION DETAILS | APPROVED | SCALE | SIZE |
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| 1 | 27/03/24 | FOR CONSTRUCTION | G. MURISON | | |
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| | | | | DESIGNED A. IBRAHIM | |
| | | | | REVIEWED R. VAN LIEROP | |

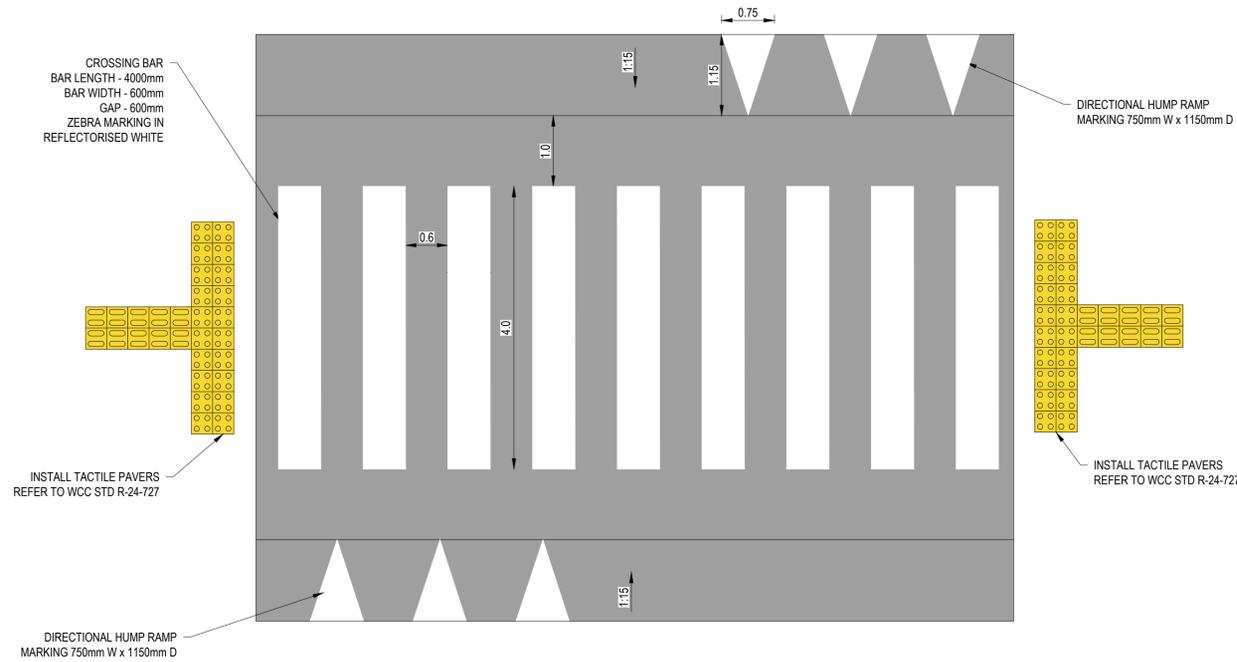
| FOR CONSTRUCTION | PROJECT | TITLE | PROJECT No. | AREA | DATE | NUMBER | REV | |
|------------------|---|--|-------------|------|------|--------|------|---|
| FOR CONSTRUCTION | KARORI TO BOTANIC GARDEN KI PĀEKAKA CONNECTIONS WCC TRANSITIONAL CYCLEWAYS | SERVICE LAYOUT PRIMARY ROUTE - IFC DESIGNS KARORI ROAD - NEAR TRINGHAM STREET SHEET 10 (OF 10) | 522550 | 0000 | DRG | DGN | 0320 | 1 |

NOTE:

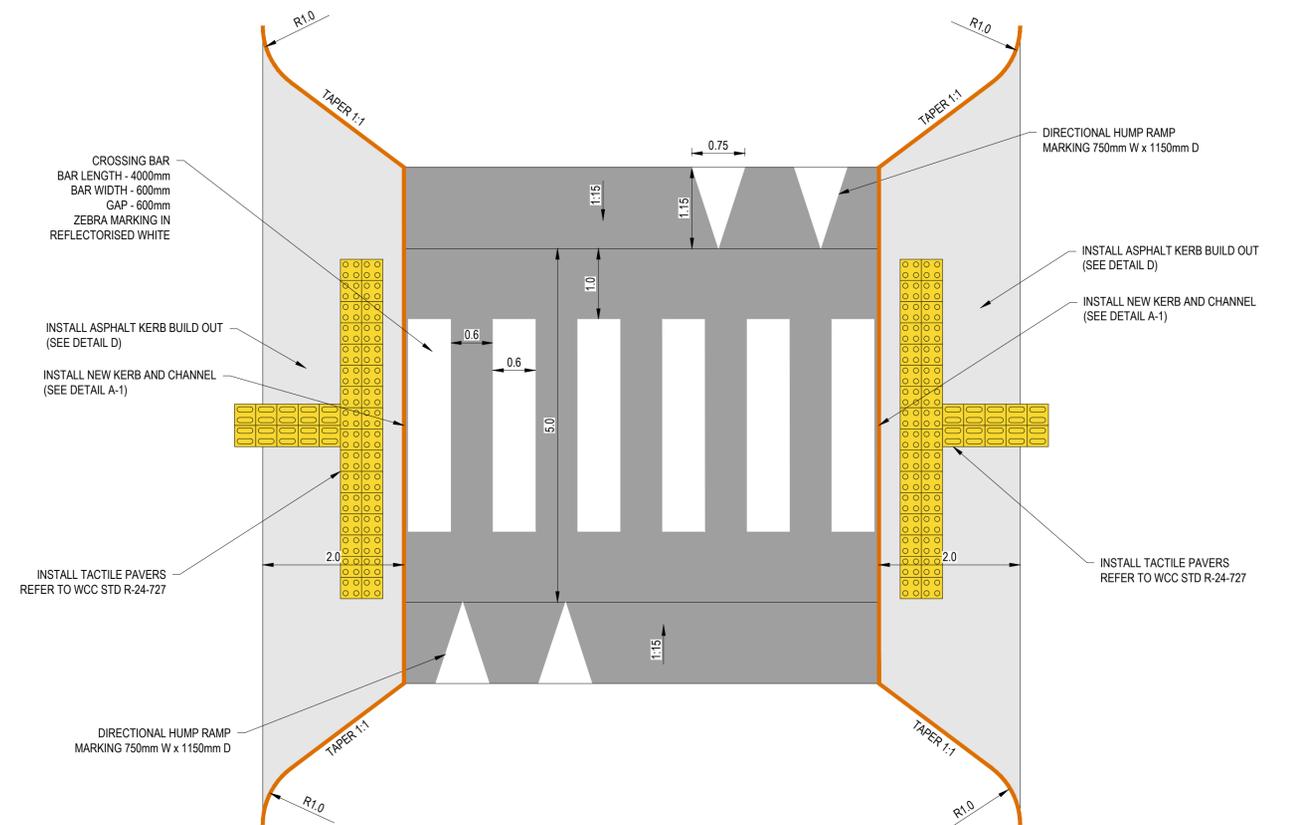
- EXISTING GROUND LINE IS INDICATIVE ONLY AND THE PROPOSED LEVELS ARE RELATIVE TO THE EXISTING GROUND.
- PROPOSED SPEED TABLE SHOULD BE 75mm IN HEIGHT AT CENTRE LINE. WIDTH AS SHOWN BELOW. CONTRACTOR TO ENSURE ADEQUATE GRADING IS PROVIDED TO CHANNEL DRAIN BEFORE COMMENCING CONSTRUCTION.
- CONTRACTOR SHALL CONSULT WITH SITE ENGINEER IF THE EXISTING PAVEMENT CONDITION IS INADEQUATE UPON EXCAVATION.
- STATIC ROLLING SHALL BE USED INSTEAD OF VIBRATION ROLLING WHERE THERE IS UNDERGROUND SERVICES CLOSE TO THE SURFACE.
- NEW KERB AND CHANNEL MUST BE LAID ON 300mm (MIN.) GAP65 SUBBASE, WHERE SUBGRADE CBR>5%. IF SUBGRADE CBR<5%, THEN ROAD MUST BE UNDERCUT AND BACKFILLED WITH AN APPROVED FILLING MATERIAL.
- APPROPRIATE DRAINAGE SHALL BE ACHIEVED ON ALL GRADING AREAS. CONTRACTOR IS RESPONSIBLE TO RESOLVE ANY IMPROPER LOW POINT WHICH IS CREATED AFTER CONSTRUCTION.



1-B TYPICAL LAYOUT OF A RAISED PEDESTRIAN CROSSING
SCALE 1:50



1-A TYPICAL LAYOUT OF A RAISED PEDESTRIAN CROSSING
SCALE 1:50



1-C TYPICAL LAYOUT OF A RAISED PEDESTRIAN CROSSING
SCALE 1:50

File Name: C:\USER\TARRYN.PETERS\AURECON\GROUP\525550 - GWM CITY STREETS TRANCHE - 3 DEVELOP\50 CHANGE\WCC TRANSITIONAL CYCLEWAYS\301 DELIVERABLES\301 DETAIL DESIGN\FC - PRIMARY ROUTE\1022560000.DWG-DGN-0500-3-TYPICAL DETAILS.DWG
 Date: 2019/03/11 10:00
 Office: Peta



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| REV | DATE | REVISION DETAILS | APPROVED |
|-----|----------|---|-----------|
| 0 | 15/12/23 | FOR CONSTRUCTION | G MURISON |
| 1 | 19/12/23 | FOR CONSTRUCTION - DETAIL 1A-1B UPDATED | G MURISON |
| 2 | 26/02/24 | FOR CONSTRUCTION - LINEMARKING UPDATE | G MURISON |
| 3 | 15/03/24 | FOR CONSTRUCTION - DETAIL 1C ADDED | G MURISON |

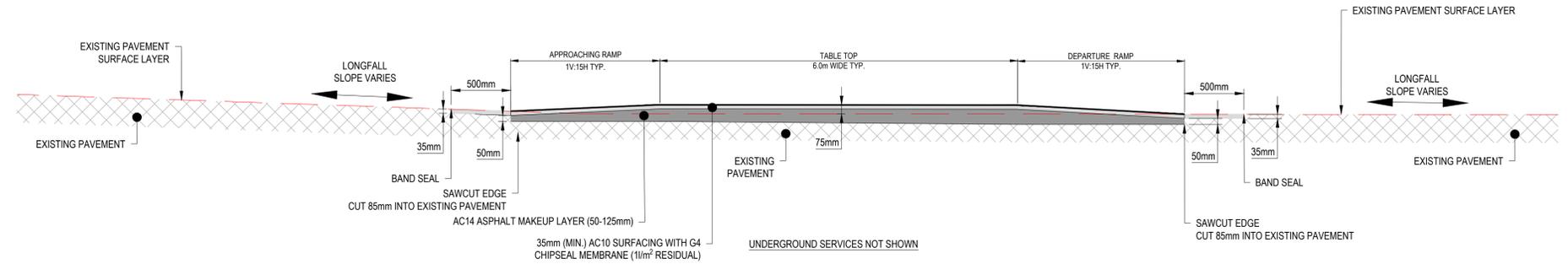
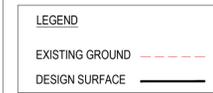
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| AS SHOWN | A1 |
| DRAWN | |
| T PETERS | |
| DESIGNED | |
| T PETERS | |
| REVIEWED | |
| S HAMILTON | |

| FOR CONSTRUCTION |
|------------------|
| APPROVED |
| DATE |

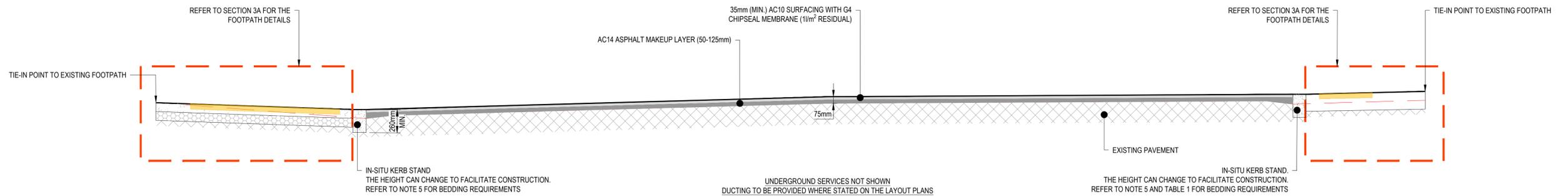
| PROJECT | KARORI TO BOTANIC GARDEN KI PAEKAKA CONNECTIONS WCC TRANSITIONAL CYCLEWAYS |
|-------------|--|
| TITLE | RAISED PEDESTRIAN CROSSING LAYOUTS TYPICAL DETAILS PRIMARY ROUTE - 100% DESIGN IFC |
| DRAWING No. | 522550 |
| PROJECT No. | 522550 |
| AREA | 0000 |
| TYPE | DRG |
| DISC | DGN |
| NUMBER | 0501 |
| REV | 3 |

NOTE:

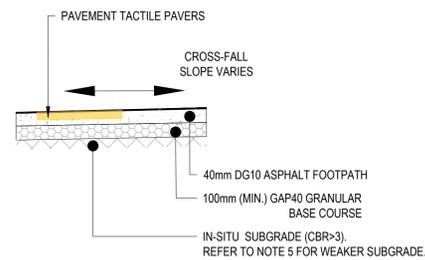
- EXISTING GROUND LINE IS INDICATIVE ONLY AND THE PROPOSED LEVELS ARE RELATIVE TO THE EXISTING GROUND.
- PROPOSED SPEED TABLE SHOULD BE 75mm IN HEIGHT AT CENTRE LINE. WIDTH AS SHOWN ON DETAIL LAYOUT SHEET (0501). CONTRACTOR TO ENSURE ADEQUATE GRADING IS PROVIDED TO CHANNEL DRAIN BEFORE COMMENCING CONSTRUCTION.
- CONTRACTOR SHALL CONSULT WITH SITE ENGINEER IF THE EXISTING PAVEMENT CONDITION IS INADEQUATE UPON EXCAVATION.
- STATIC ROLLING SHALL BE USED INSTEAD OF VIBRATION ROLLING WHERE THERE IS UNDERGROUND SERVICES CLOSE TO THE SURFACE.
- NEW KERB AND CHANNEL MUST BE LAID ON 300mm (MIN.) GAP65 SUBBASE, WHERE SUBGRADE CBR>5%. IF SUBGRADE CBR<5%, THEN ROAD MUST BE UNDERCUT AND BACKFILLED WITH AN APPROVED FILLING MATERIAL.
- APPROPRIATE DRAINAGE SHALL BE ACHIEVED ON ALL GRADING AREAS. CONTRACTOR IS RESPONSIBLE TO RESOLVE ANY IMPROPER LOW POINT WHICH IS CREATED AFTER CONSTRUCTION.



2 TYPICAL SECTION ALONG THE CENTRE LINE OF THE ROAD



3 TYPICAL SECTION ALONG THE RAISED TABLE
SCALE 1:25



3A NEW FOOTPATH NEXT TO THE RAISED TABLE
SCALE 1:25

File Name: C:\USER\TERRY\PROJECTS\AURECON\GROUP\52550 - COMM CITY STREETS TRANCHE - 3 DEVELOP\305 CHANGE\WCC TRANSITIONAL CYCLEWAYS\303 DELIVERABLES\303 TYPICAL DETAILS.DWG
 Plot Date: 2019/03/28 11:02:00
 Office Path



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| REV | DATE | REVISION DETAILS | APPROVED |
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| 2 | 27/03/24 | FOR CONSTRUCTION | G MURISON |

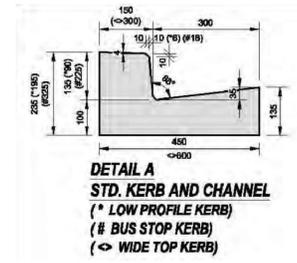
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|-----------------|------|
| AS SHOWN | A1 |
| DRAWN | |
| T PETERS | |
| DESIGNED | |
| T PETERS | |
| REVIEWED | |
| S HAMILTON | |

| FOR CONSTRUCTION |
|------------------|
| APPROVED |
| DATE |

| PROJECT | KARORI TO BOTANIC GARDEN KI PAEKAKA CONNECTIONS WCC TRANSITIONAL CYCLEWAYS |
|-------------|--|
| TITLE | RAISED PEDESTRIAN CROSSING SECTIONS TYPICAL DETAILS PRIMARY ROUTE - IFC DESIGN |
| DRAWING No. | 522550 - 0000 - DRG - DGN - 0502 - 2 |

NOTE:

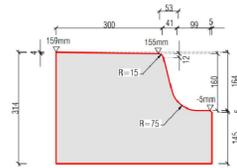
- STANDARD KERB SHALL BE LAID ON 300mm (MIN) GAP65 SUBBASE IN ROADS AND 100mm GAP40 IN FOOTPATHS (WHERE SUBGRADE CBR>5) IF THE SUBGRADE CBR<5 THEN ROADS AND FOOTPATH MUST BE UNDERCUT AND BACKFILLED WITH FILLING MATERIALS.
- TACTILE GROUND INDICATORS MUST BE INSTALLED IN ACCORDANCE WITH:
 - NZTA RTS 14: GUIDELINE FOR FACILITIES FOR BLIND AND VISION-IMPAIRED PEDESTRIANS
 - ASINZS 1428.4:2009 DESIGN FOR ACCESS AND MOBILITY
 - WAKA KOTAHĪ 20-20 TACTILE INDICATOR INSTALLATION NOTE
- REFER TO WCC CODE OF PRACTICE FOR LAND DEVELOPMENT FOR TACTILE BEDDING PROPERTIES.
- PRAM CROSSING RAMP AND FLARING SHOULD BE CONSTRUCTED IN CONTRASTING COLOUR / OR TEXTURE TO THE ADJACENT FOOTPATH.
- GRANULAR SUBGRADE IMPROVEMENT MATERIAL CAN BE GAP40, GAP65, OR ANY APPROPRIATE MATERIAL WITH CBR<5%. IMPROVEMENT LAYER SHALL BE COMPACTED IN LAYER(S), WITH LAYER THICKNESS BETWEEN 2.5 AND 4 TIMES OF THE NOMINAL AGGREGATE SIZE.
- EXTEND THE PAVEMENT SURFACE 300mm FOR THE TRAVERSE JOINTS AND 500mm FOR THE LONGITUDINALLY POSITIONED JOINTS TO BE LOCATED OUTSIDE OF WHEELPATHS.



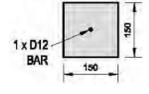
A-1 STANDARD KERB
SCALE 1:10



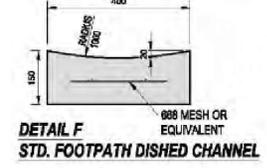
A-2 MOUNTABLE KERB
SCALE 1:10



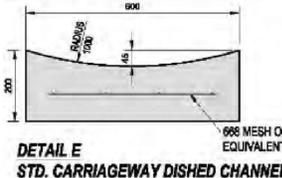
A-3 KASSEL KERB
SCALE 1:10



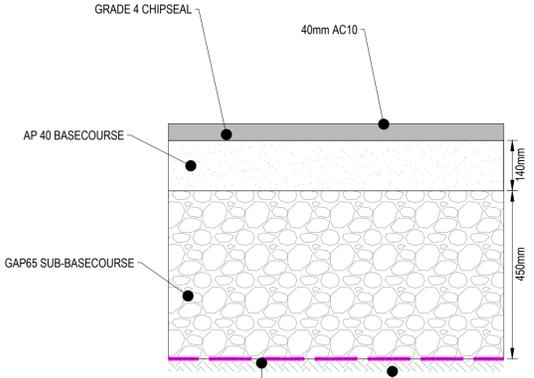
A-4 STANDARD NIB
SCALE 1:10



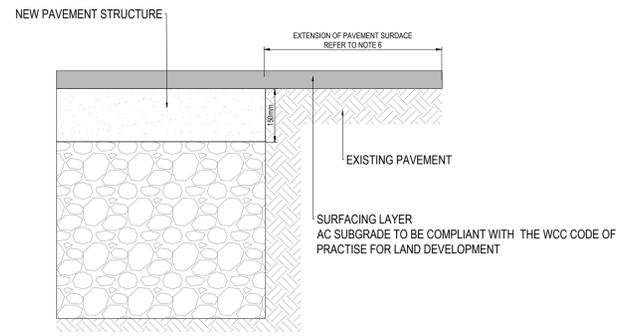
B-1 FOOTPATH DISHED CHANNEL
SCALE 1:10



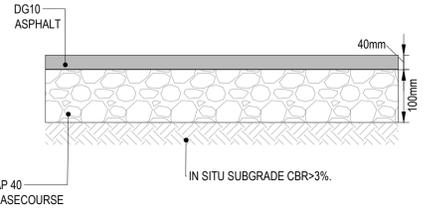
B-2 CARRIAGEWAY DISHED CHANNEL
SCALE 1:10



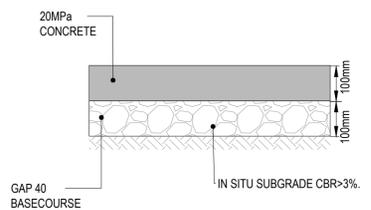
C ROAD PAVEMENT DETAIL
SCALE 1:10



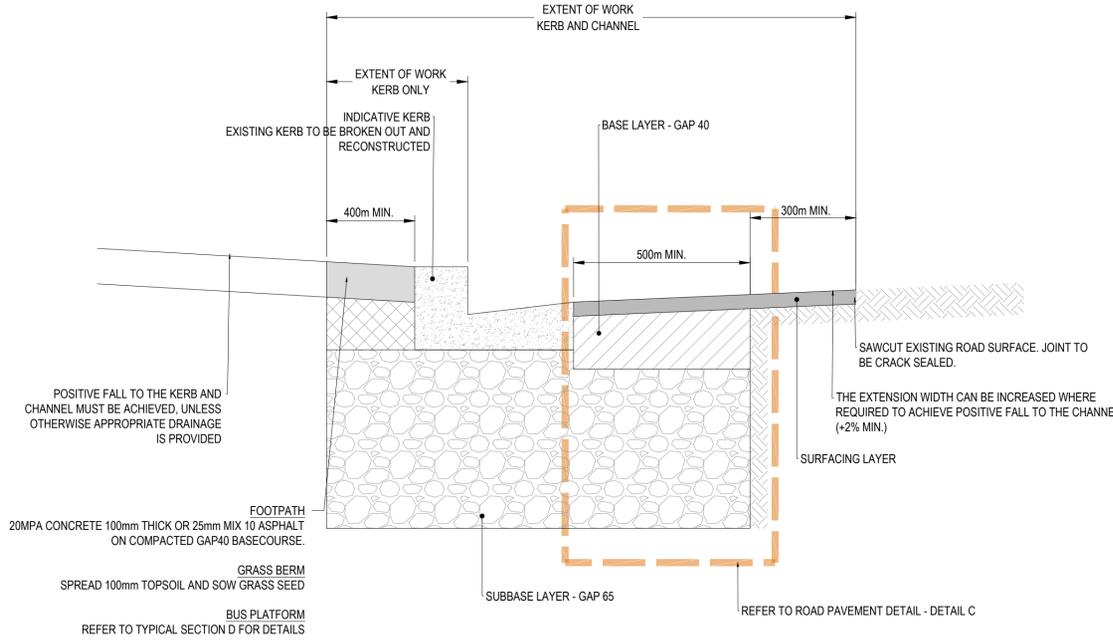
C-1 PAVEMENT JOINTS
SCALE 1:10



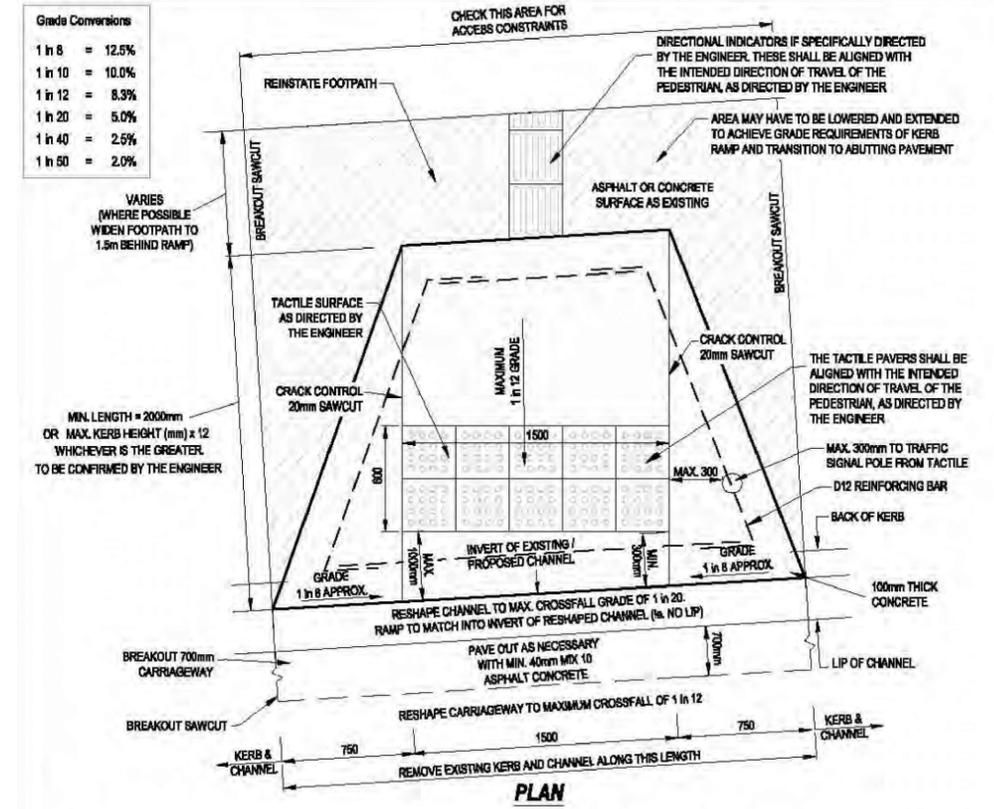
D ASPHALT FOOTPATH PAVEMENT
SCALE 1:10



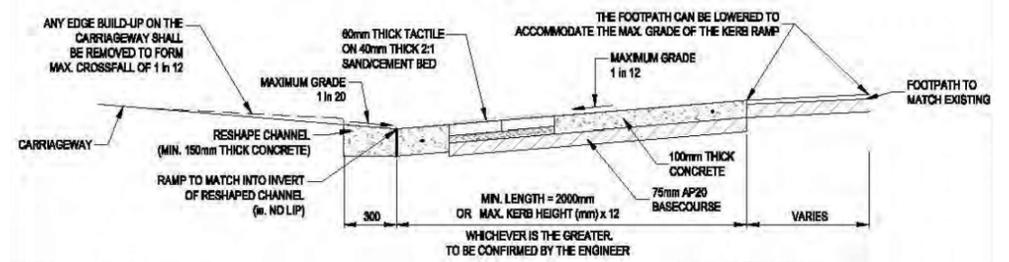
E CONCRETE FOOTPATH PAVEMENT
SCALE 1:10



F KERB AND CHANNEL AND KERB ONLY REPLACEMENT
SCALE 1:10



G PEDESTRIAN RAMP WITH TACTILE PAVERS - PLAN
SCALE 1:20



G-1 PEDESTRIAN RAMP WITH TACTILE PAVERS - SECTION
SCALE 1:20

File Name: C:\USERS\TERRY\FETERS\AURECON\GROUP\22595 - LOWW CITY STREETS TRANCHIE - 3 DEVELOP\305 CHANGE\WCC TRANSITIONAL CYCLEWAYS\50 DELIVERABLES\7 DETAIL DESIGN\3 - PRIMARY ROUTES\2525550-0000 DRG-DGN-000-01 TYPICAL DETAILS.DWG
 Plot Date: 2018-02-28 11:02:00
 Office Path:



| REV | DATE | REVISION DETAILS | APPROVED | SCALE | SIZE |
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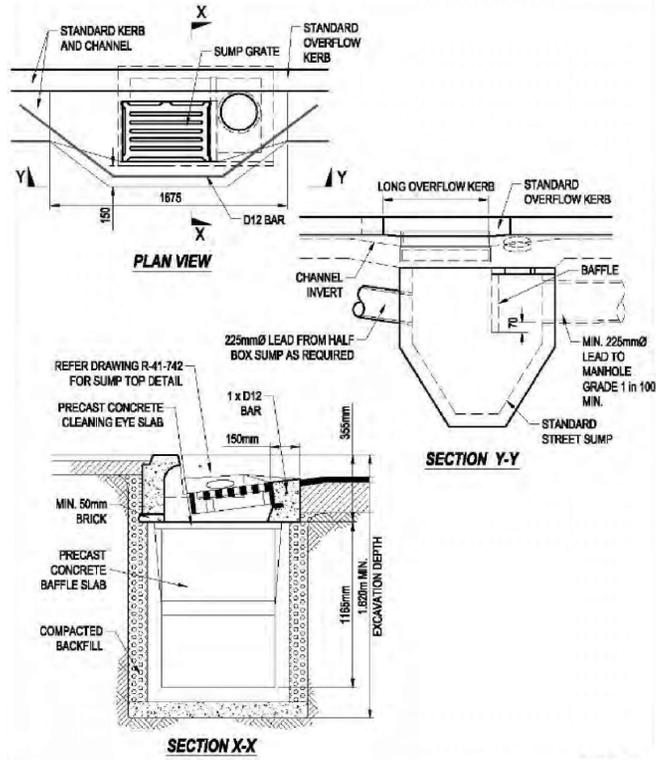
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| | | T PETERS | |
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| | | T PETERS | |
| | | REVIEWED | |
| | | S HAMILTON | |

| FOR CONSTRUCTION | PROJECT | TITLE |
|------------------|--|---|
| | KARORI TO BOTANIC GARDEN KI PAEKAKA CONNECTIONS WCC TRANSITIONAL CYCLEWAYS | KERBS, PAVEMENTS, PEDESTRIAN RAMP TYPICAL DETAILS |
| | | PRIMARY ROUTE - 100% DESIGN IFC |
| | | |

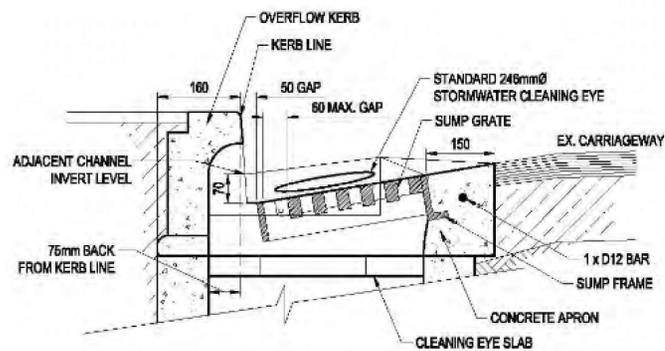
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|-------------|-------------|------|------|------|--------|-----|
| 522550 | 0000 | DRG | DGN | 0504 | 1 | |

NOTE:

- APPROVED CYCLE FRIENDLY GRATE SHALL BE USED. THE APRON OF THE CATCHPIT MUST BE RESHAPED TO FOLLOW EXISTING ROAD SURFACE GRADING AND THE FRAME MUST NOT BE MORE THAN 5mm BELOW THE LEVEL OF THE ROAD.
- WHERE EXISTING SERVICES ARE DAMAGED AS A RESULT OF TRENCHING WORK, CONTRACTOR SHALL IMMEDIATELY ADVISE ENGINEER ON SITE AND THE OWNER OF THE DAMAGED SERVICE TO ARRANGE FOR REPAIRS TO BE CARRIED OUT ON CONTRACTOR'S COST BEFORE BACKFILLING. IT IS THE CONTRACTOR RESPONSIBILITY TO IDENTIFY THE EXISTING UNDERGROUND SERVICES BEFORE TRENCHING WORKS.
- SELECTED FILL MATERIAL IS GENERALLY AP40 OR AP65, BUT IT MAY DIFFER IN CERTAIN AREAS IN ACCORDANCE WITH RCA OR OTHER REQUIREMENTS
- FOR TRENCH REINSTATEMENT IN PAVEMENTS, ALL BACKFILL MATERIAL SHALL BE PLACED BACK INTO THE TRENCH IN 100-200mm THICK LAYERS AND COMPACTED USING SUITABLE COMPACTION EQUIPMENT.
- WHERE A TRENCH IS ON CONCRETE, ASPHALT, OR CHIPSEAL SURFACE, THE EDGE OF TRENCH SHALL BE CUT WITH A POWER SAW PRIOR TO THE EXCAVATION OF THE TRENCH. THE CUT IS TO EXTEND THROUGH THE FULL THICKNESS OF THE SURFACE LAYER IN A CLEAN STRAIGHT PARALLEL LINE.
- ALL BEDDING AND CUSHION MATERIAL MUST BE FREE DRAINING, NON-COMPRESSIBLE GRANULAR METRICAL.
- ROAD SURFACE JOINT ARE TO BE SEALED WITH APPROVED SEALANT.
- WHERE STABILISED LAYERS EXIST UNDER THE PAVEMENT, THE TRENCH SHALL BE REINSTATED WITH SIMILAR METRICAL. USING OTHER MATERIAL SHALL BE APPROVED BY THE ENGINEER.
- SEE WELLINGTON WATER REGIONAL SPECIFICATION FOR WATER SERVICES DRG-01 FOR MANHOLE INSTALLATION DETAILS
- SET OUT OF CATCHPIT AND CATCHPIT MANHOLES TO BE CONFIRMED BY CONTRACTOR ON SITE TO ENSURE
 - LOCATED AT LOW POINTS ON THE CARRIAGEWAY;
 - TO SUIT KERB LEVELS AND POSITION; AND
 - POTHOLED SERVICES
- HALF BOX SUMP:
 - HALF BOX SUMP IS TO MATCH WELLINGTON CITY COUNCIL STANDARD SUMP REQUIREMENTS. ADDITIONAL REQUIREMENTS FOR HALF BOX SUMP ARE NOTED.
 - HALF BOX SUMP LID LEVEL IS TO BE AT LEAST 150mm LOWER THAN THE UPSTREAM INLET SUMP LID LEVEL.
 - ALL CONNECTIONS TO THE HALF BOX SUMP SYSTEM ARE TO BE SEALED.
- SUMPS ARE TO BE INSTALLED IN ACCORDANCE WITH WELLINGTON CITY COUNCIL'S CODE OF PRACTICE

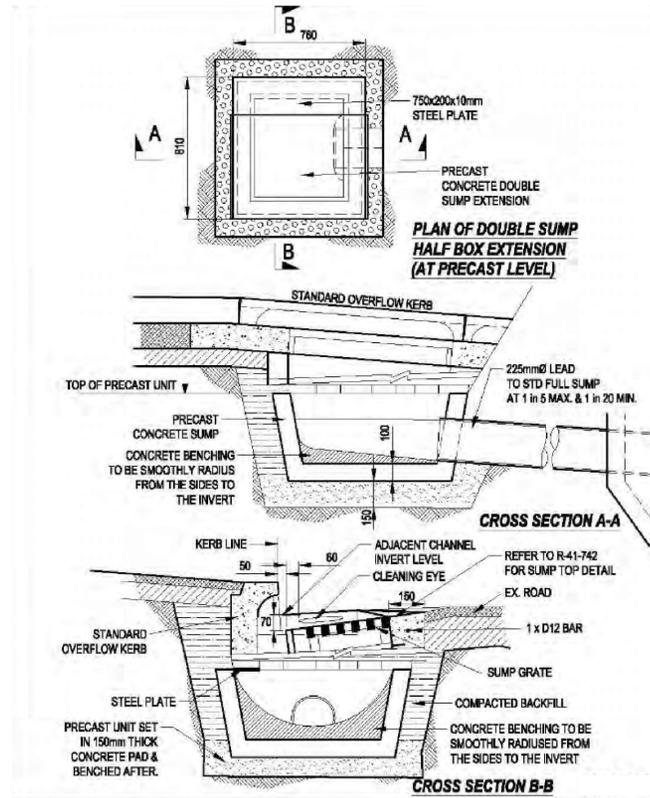


H WCC SUMP AND SECTION
SCALE 1:25



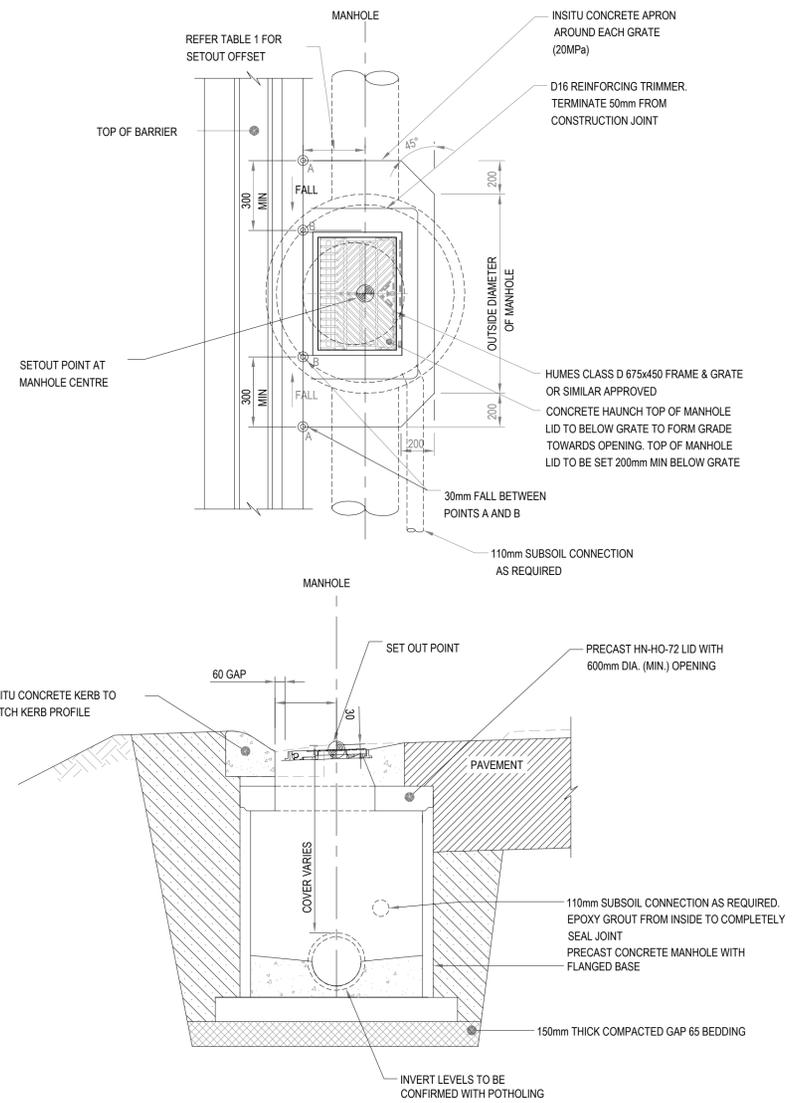
REFER TO PLAN NO. R-41-742 IN WCC CODE OF PRACTICE FOR LAND DEVELOPMENT FOR DETAILS

H-1 TOP OF SUMP
SCALE 1:10

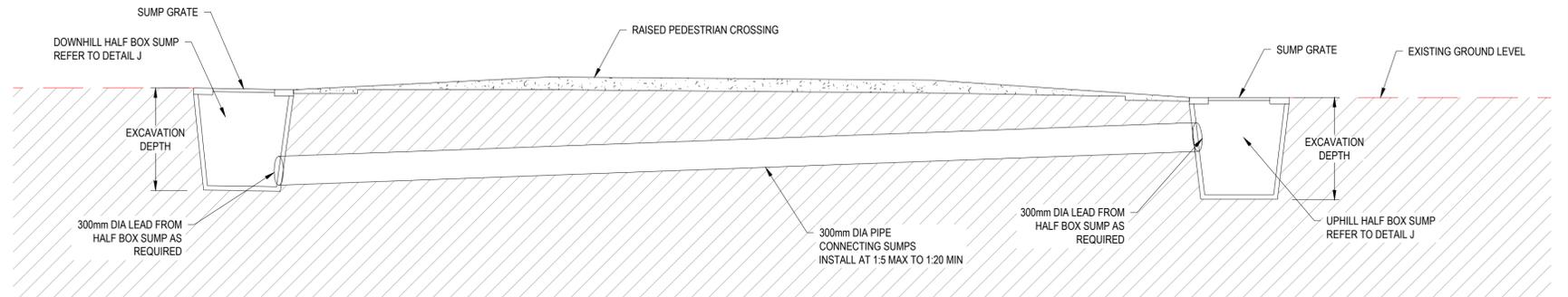


REFER TO PLAN NO. R-41-741 IN WCC CODE OF PRACTICE FOR LAND DEVELOPMENT FOR DETAILS

J WCC HALF BOX SUMP EXTENSION AND SECTION
SCALE 1:20



K CATCHPIT MANHOLE
SCALE 1:20



I TYPICAL SECTION OF A HALF BOX SUMP
SCALE 1:25

File Name: C:\USERS\TARYN\FETERS\AURECON\GROUP\22595 - LOWM CITY STREETS TRANSDIE - 3 DEVELOP\305 CHANGE\WCC TRANSITIONAL CYCLEWAYS\SD DELIVERABLES\7 DETAIL DESIGN\100% DESIGN - PRIMARY ROUTE\TES2550-0000\PRECON\0004-TYPICAL DETAILS.DWG
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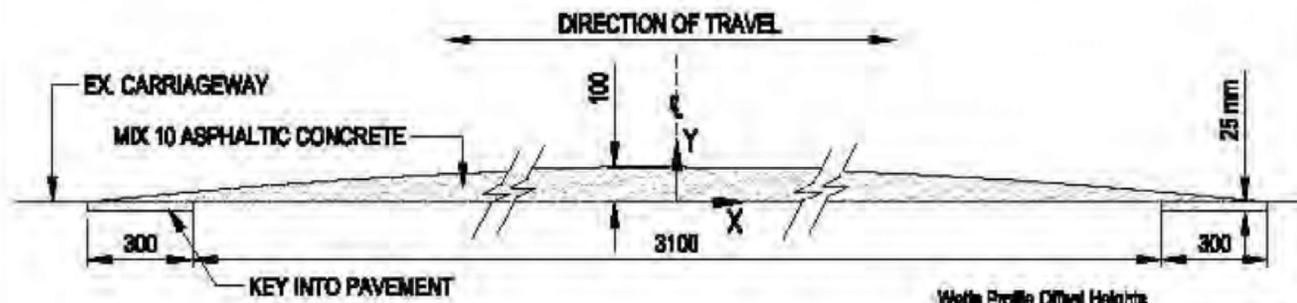
CLIENT
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| REV | DATE | REVISION DETAILS | APPROVED |
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| 0 | 15/12/23 | FOR CONSTRUCTION | G MURISON |

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| REVIEWED | |
| S HAMILTON | |

| FOR CONSTRUCTION |
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| APPROVED |
| DATE |

| PROJECT | TITLE | | | | | |
|--|--|-------------|-------------|-------------|---------------|------------|
| KARORI TO BOTANIC GARDEN KI PAKAKA CONNECTIONS WCC TRANSITIONAL CYCLEWAYS | SUMPS, HALF BOX SUMP TYPICAL DETAILS PRIMARY ROUTE - 100% DESIGN IFC | | | | | |
| DRAWING No. | PROJECT No. | AREA | TYPE | DISC | NUMBER | REV |
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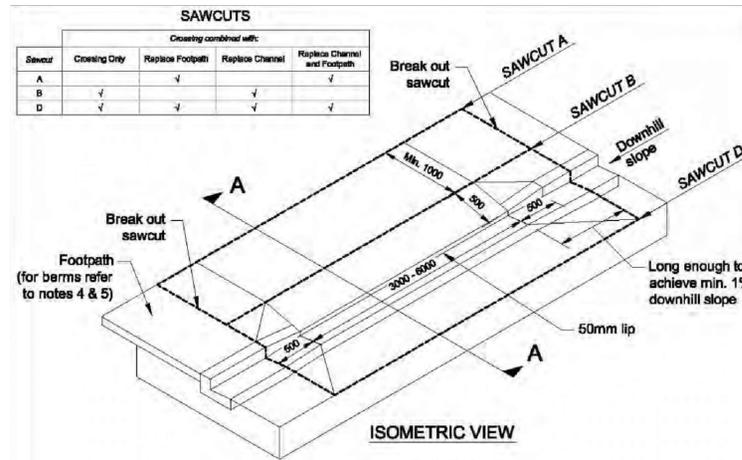
WATTS PROFILE

Watts Profile Official Heights

| X (m) | Y (mm) | X (m) | Y (mm) | X (m) | Y (mm) |
|-------|--------|-------|--------|-------|--------|
| 0.0 | 100 | 0.7 | 86 | 1.4 | 63 |
| 0.1 | 90 | 0.8 | 81 | 1.5 | 54 |
| 0.2 | 88 | 0.9 | 79 | 1.6 | 44 |
| 0.3 | 97 | 1.0 | 71 | 1.7 | 16 |
| 0.4 | 95 | 1.1 | 65 | 1.8 | 0 |
| 0.5 | 85 | 1.2 | 58 | | |
| 0.6 | 90 | 1.3 | 61 | | |

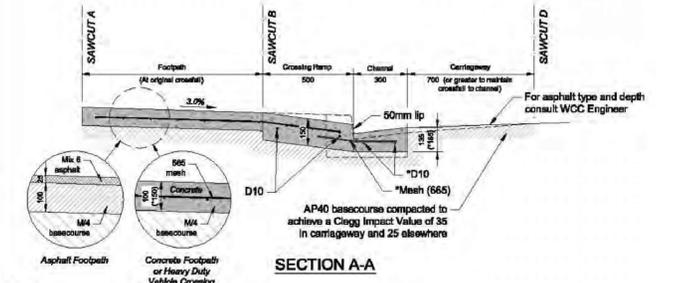
REFER TO PLAN NO. R-45-702 IN WCC CODE OF PRACTICE FOR LAND DEVELOPMENT FOR DETAILS

N WATTS PROFILE SPEED HUMP
SCALE 1:10



REFER TO PLAN NO. R-24-722 IN WCC CODE OF PRACTICE FOR LAND DEVELOPMENT FOR DETAILS

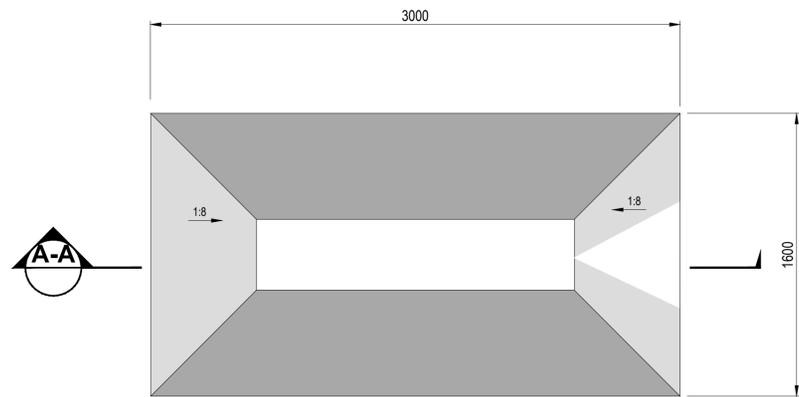
O VEHICLE CROSSING
NTS



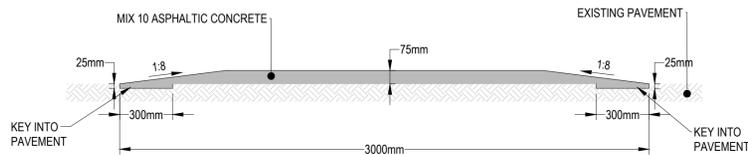
NOTES:
 1) WHERE KERB HEIGHT EXCEEDS 135mm REFER TO ENGINEER.
 2) 20 MPa (80-120 SLUMP) CONCRETE WITH LIGHTLY BROOMED FINISH.
 3) SOFT SUBSOILS TO BE UNDERCUT BY 200mm AND FILLED WITH COMPACTED BASECOURSE.
 4) CROSSING DETAILS SHOWN ALSO APPLY WHERE NO PUBLIC FOOTPATH EXISTS.
 5) WHERE A GRASS BERM SEPARATES THE FOOTPATH FROM THE KERB, THE CROSSING RAMP SHALL EXTEND FROM THE KERBLINE TO THE EDGE OF THE ADJACENT FOOTPATH, OR 500mm, WHICHEVER IS GREATER. THE SPREAD LENGTH ALONG THE KERBLINE EITHER SIDE OF THE CROSSING SHALL BE 500mm.
 6) * REFERS TO HEAVY DUTY VEHICLE CROSSINGS

REFER TO PLAN NO. R-24-722 IN WCC CODE OF PRACTICE FOR LAND DEVELOPMENT FOR DETAILS

O-A VEHICLE CROSSING - SECTION A-A
NTS



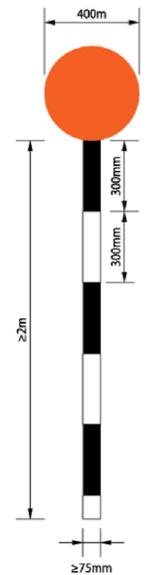
P ASPHALT SPEED CUSHION
SCALE 1:20



P-1 SECTION A-A
SCALE 1:20



R NEW MEDIAN ISLANDS AT CURTIS STREET INTERSECTION
1:100



S TYPICAL BELISHA BEACON
1:15

File name: C:\USERS\TARYN\PETERS\AURECON\GROUP\22595 - LOWM CITY STREETS TRANCHIE - 3 DEVELOP\305 CHANGE\WCC TRANSITIONAL CYCLEWAYS\20 DELIVERABLES\307 DETAIL DESIGN\1004 DESIGN - PRIMARY ROUTES\25250-0000\DRG-CON-0000-01.TYPICAL DETAILS.DWG
 Plot Date: 2018-02-28 11:02:00
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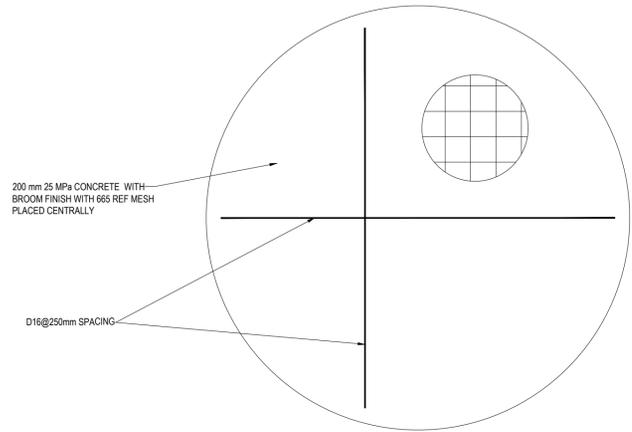
CLIENT
**Absolutely Positively
 Wellington City Council**
 Me Heke Ki Pōneke

| REV | DATE | REVISION DETAILS | APPROVED |
|-----|----------|------------------|-----------|
| 0 | 15/12/23 | FOR CONSTRUCTION | G MURISON |

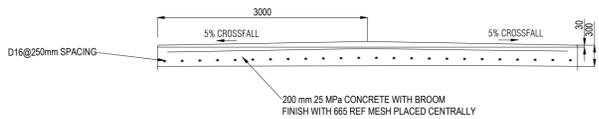
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| DRAWN | |
| T PETERS | |
| DESIGNED | |
| T PETERS | |
| REVIEWED | |
| S HAMILTON | |

| FOR CONSTRUCTION |
|------------------|
| APPROVED |
| DATE |

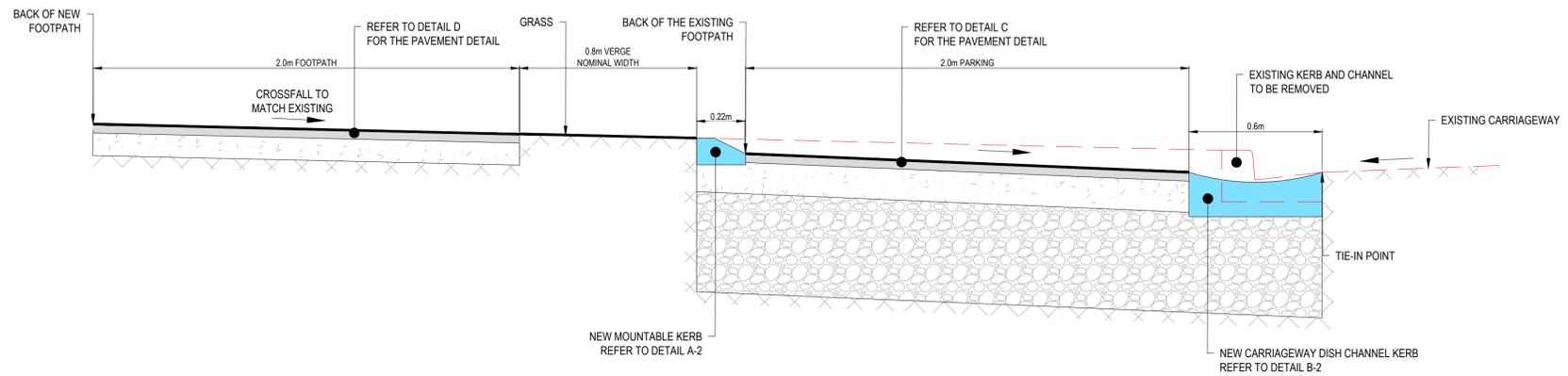
| PROJECT | KARORI TO BOTANIC GARDEN KI PAKAKA CONNECTIONS WCC TRANSITIONAL CYCLEWAYS |
|-------------|---|
| TITLE | WATTS SPEED HUMP, VEHICLE CROSSING, SPEED CUSHION TYPICAL DETAILS PRIMARY ROUTE - 100% DESIGN IFC |
| DRAWING No. | 522550 |
| PROJECT No. | 522550 |
| AREA | 0000 |
| TYPE | DRG |
| DISC | DGN |
| NUMBER | 0506 |
| REV | 0 |



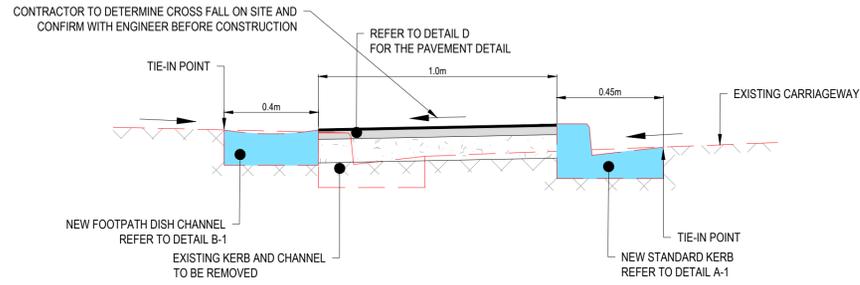
T MOUNTABLE TRAFFIC CIRCLE - PLAN
1:50



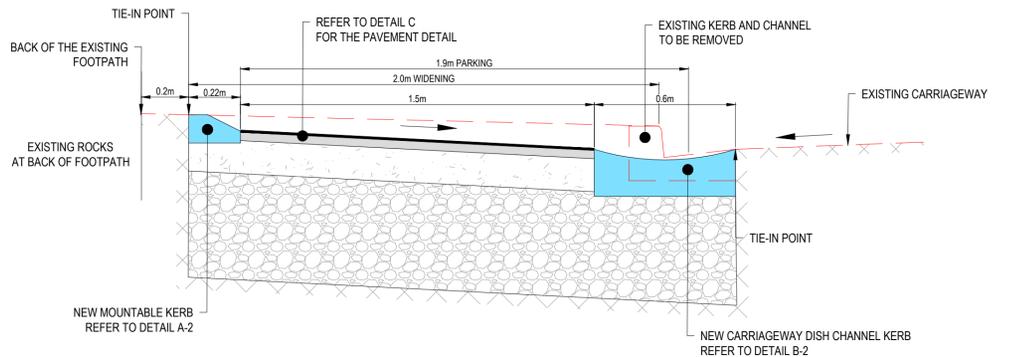
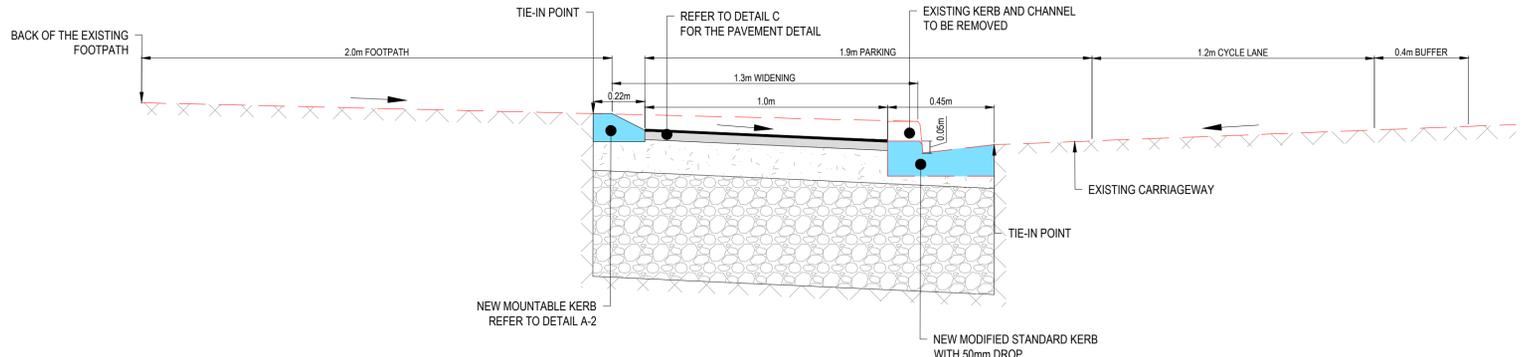
T-1 MOUNTABLE TRAFFIC CIRCLE - CROSS SECTION
1:50



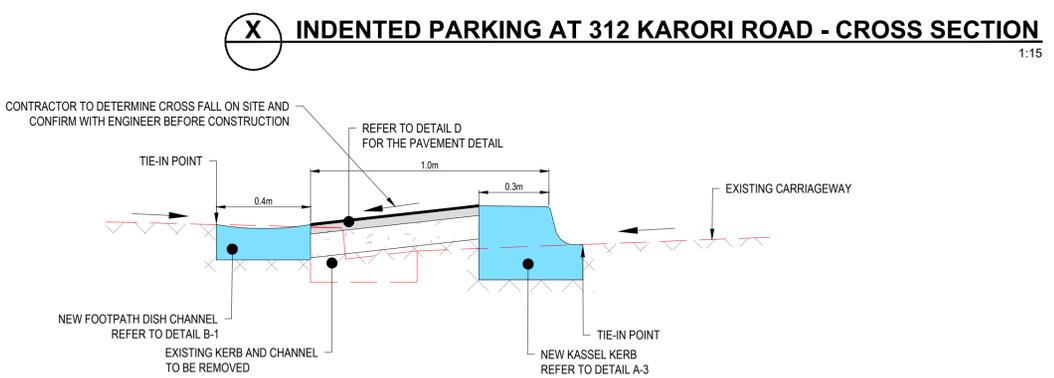
U INDENTED PARKING AT 152 GLENMORE STREET - CROSS SECTION
1:15



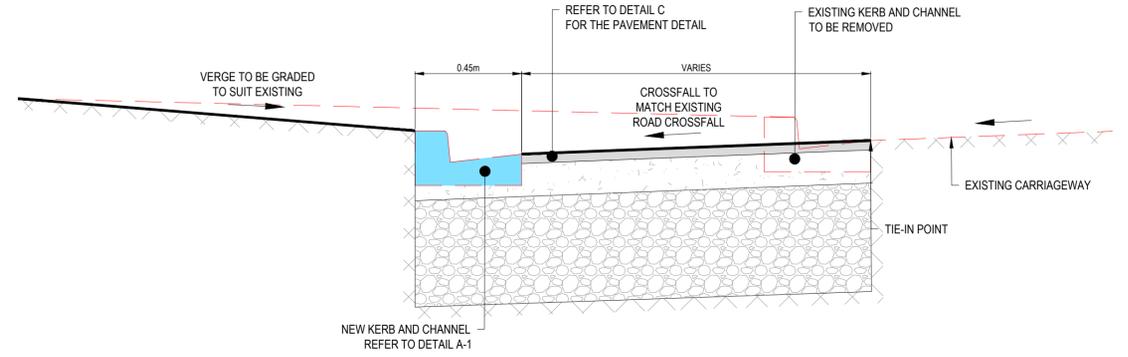
V KERB BUILD OUT FOR BUS STOP AT 139 KARORI ROAD - CROSS SECTION
1:15



W INDENTED PARKING AT 181 GLENMORE STREET - CROSS SECTION
1:15



X INDENTED PARKING AT 312 KARORI ROAD - CROSS SECTION
1:15



Z HORSESHOE BEND WIDENING - CROSS SECTION
1:15

Y KERB BUILD OUT FOR BUS STOP AT 247 KARORI ROAD - CROSS SECTION
1:15

File name: C:\Users\TERRY\PETERS\AURECON GROUP\2259 - LGW\WCC\STREETS STRANDE - 3 DEVELOPERS CHANGE\WCC TRANSITIONAL CYCLEWAYS\DELIVERABLES\07 DETAIL DESIGN\FC - PRIMARY ROUTE\522550\0000 DRG-DGN-0500-1 TYPICAL DETAILS.DWG
 Plot Date: 2018/02/28 11:20:00
 Office: Pth



| REV | DATE | REVISION DETAILS | APPROVED | SCALE | SIZE |
|-----|----------|------------------|-----------|----------|------|
| 0 | 27/03/24 | FOR CONSTRUCTION | G MURISON | AS SHOWN | A1 |

| | | |
|------------------|-----------|------|
| FOR CONSTRUCTION | APPROVED | DATE |
| DRAWN | T PETERS | |
| DESIGNED | T PETERS | |
| REVIEWED | M FRANCIS | |

| PROJECT | KARORI TO BOTANIC GARDEN KI PAEKAKA CONNECTIONS WCC TRANSITIONAL CYCLEWAYS | |
|-------------|---|------|
| TITLE | KERB CHANGES CROSS SECTIONS TYPICAL DETAILS PRIMARY ROUTE - IFC DESIGN | |
| DRAWING No. | 522550 | 0000 |
| PROJECT No. | 522550 | 0000 |
| AREA | DRG | DGN |
| TYPE | DRG | DGN |
| DISC | 0508 | 0 |
| NUMBER | 0508 | 0 |
| REV | 0 | 0 |